

RESOLUTION NO. 2014-157

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ROHNERT PARK,
CALIFORNIA, APPROVING A SPECIFIC PLAN FOR THE NORTHWEST SPECIFIC
PLAN AREA LOCATED SOUTH OF MILLBRAE AVENUE, WEST OF DOWDELL
AVENUE, NORTH OF BUSINESS PARK DRIVE, AND EAST OF LANGNER AVENUE
IN SONOMA COUNTY, CA**

WHEREAS, the applicant, the City of Rohnert Park, filed Planning Applications proposing a General Plan Amendment (PLGP2014-0006), Specific Plan (PLSP2014-0004), Rezoning (PLRZ2014-0003), Annexation (PLAX2014-0001), and certification of the Final Environmental Impact Report ("EIR") (PLEN2014-0004), in connection with the proposed Northwest Specific Plan Project ("Project"), in accordance with the City of Rohnert Park Municipal Code ("RPMC"); and

WHEREAS, on September 11, 2014 the Planning Commission held public hearings at which time interested persons had an opportunity to testify either in support or opposition to the proposal; and

WHEREAS, at the September 11, 2014 hearing, the Planning Commission reviewed and considered the Northwest Specific Plan ("NWSP" or "Specific Plan") (**Exhibit A**) and recommended its approval by the City Council; and

WHEREAS, the City Council certified the Final EIR prepared for the Project; and has otherwise carried out all requirements of the California Environmental Quality Act ("CEQA"); and

WHEREAS, pursuant to California State Law and the Rohnert Park Municipal Code, public hearing notices were mailed to all property owners within an area exceeding a 300 foot radius of the subject property and a public hearing was published for a minimum of 10 days prior to the public hearing in the Community Voice; and

WHEREAS, subsequent to the September 11, 2014 Planning Commission hearing, the City made text revisions, for clarification purposes, to pages 94 and 122 of the Specific Plan, which are reflected **Exhibit B** attached to this Resolution; and

WHEREAS, on November 25, 2014 the City Council held public hearings at which time interested persons had an opportunity to testify either in support or opposition to the proposal; and

WHEREAS, the City Council has reviewed and considered the information contained in Planning Application No. PLSP2014-0004 for the proposed NWSP; and

WHEREAS, the City Council of the City of Rohnert Park makes the following findings:

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Rohnert Park makes the following findings, determinations and recommendations with respect to the proposed NWSP:

Section 1. That the above recitations are true and correct and material to this Resolution.

Section 2. The City Council certified the Final EIR for this Project, including adoption of associated CEQA Findings, Statement of Overriding Considerations, and the Mitigation Monitoring and Reporting Program, as described in City Council Resolution No. 2014-155, approved on November 25, 2014 concurrently with the City Council's approval of this Resolution.

Section 3. Findings. The City Council hereby makes the following findings concerning the NWSP proposed by Planning Application No. PLSP2014-0004 pursuant to Rohnert Park Municipal Code section 17.06.390:

1. *The specific plan or specific plan amendment is consistent with the city's General Plan.*

Criteria Satisfied. The NWSP would implement the General Plan's goals for the NWSP area as described in the Community Design Element. It would further implement the General Plan in that it would increase the City's existing housing stock. The NWSP proposes to meet the goals of the General Plan by providing for a mix of commercial, industrial, residential, and park uses.

2. *The specific plan or specific plan amendment will not adversely affect the public health and safety or result in incompatible land uses.*

Criteria Satisfied. The NWSP is not expected to have negative impacts on the health or well-being of area residents or occupants of the surrounding land uses. The proposed neighborhood is in character with surrounding neighborhoods. Mixed-use and commercial uses are planned on either side of Golf Course Drive West and adjacent to the Graton Rancheria Casino. The proposed commercial development and park/recreational facilities would benefit existing residents in the area, as well as new residents of the NWSP area.

3. *The specific plan or specific plan amendment provides the framework to phase and pace growth within the specific plan area so as to ensure completion of all necessary public facilities concurrently with completion of the specific plan; or, alternately, a statement of public policy consideration can be adopted and/or an exception granted in accordance with general plan policies GM-10, GM-11, and GM-12.*

Criteria Satisfied. The NWSP will be developed over a period of approximately 20 years, as described in the NWSP. However, there is no physical reason for the NWSP to be developed in phases. Due to a difference in ownership patterns on the north and south side Golf Course Drive West, it is likely that the south district will develop first and as a single project; whereas, the north district will likely develop later as several individual projects. The NWSP is designed to provide for adequate infrastructure and to be integrated with existing City public

improvements. The timing of the construction of public facilities will be paced to meet the needs of the development.

4. *The specific plan or specific plan amendment identifies adequate financing mechanisms for the infrastructure and public facilities required to support the development.*

Criteria Satisfied. The NWSP identifies several options for funding capital improvements given the different development scenarios that may occur. Future projects within the NWSP area will be subject to the payment of fees to provide for and maintain public infrastructure such as Public Facilities Financing Plan fees to help fund off-site improvements, City-wide and Regional Traffic fees, Public Services Impact fees, and Maintenance of on-site Infrastructure fees. The public improvements will be funded through a Funding Mechanism such as a community facilities district or assessment district. The NWSP provides that Development Agreements will be utilized with future development proposals to ensure adequate financial resources are in place for needed public services and infrastructure.

5. *The proposed specific plan is consistent with the planning and prezoning designation provided for the specific plan area, pursuant to Government Code Sections 65300 and 65859, or alternatively functions as prezoning for the unincorporated territory.*

Criteria Satisfied. The NWSP area will be prezoned “Specific Plan District (SP),” which is consistent with the direction of the City’s General Plan. Therefore the proposed NWSP is consistent with the prezoning designation of SP which zoning designation shall become effective at the same time the annexation becomes effective.

Section 4. A duly noticed public hearing on the proposed NWSP was held on November 25, 2014.

NOW, THEREFORE, BE IT FURTHER RESOLVED, THAT the City Council of the City of Rohnert Park does hereby adopt the Findings stated hereinabove and approve Application No. PLSP2014-0004 for the **Northwest Specific Plan** as provided at **Exhibit A** and the proposed revisions as provided at **Exhibit B**, in their entirety, both attached hereto and incorporated by this reference.


DULY AND REGULARLY ADOPTED on this 25th day of November, 2014.



CITY OF ROHNERT PARK

Joseph T. Callinan, Mayor

ATTEST:


JoAnne Buergler, City Clerk

Attachment: Exhibit A and Exhibit B

BELFORTE: ABSENT MACKENZIE: AYE STAFFORD: AYE AHANOTU: AYE CALLINAN: AYE
AYES: (4) NOES: (0) ABSENT: (1) ABSTAIN: (0)

Exhibit A
Resolution 2014-157

City of Rohnert Park **NORTHWEST SPECIFIC PLAN**



**Adopted November 25, 2014
City Council Resolution No. 2014-157**



City of Rohnert Park
NORTHWEST SPECIFIC PLAN

Adopted November 25, 2014
City Council Resolution No. 2014-157



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Chapter One

INTRODUCTION

The Northwest Specific Plan Area is identified in the City's General Plan as an expansion area for the City. The General Plan called for a Specific Plan to be developed for this area ahead of eventual annexation by the City of Rohnert Park. The Northwest Specific Plan Area, currently under the jurisdiction of Sonoma County, is within Rohnert Park's Sphere of Influence, meaning that it is land intended to eventually be annexed by the City.

The Rohnert Park General Plan states that a Specific Plan process should be undertaken for the Northwest Area to plan in more detail for a variety of land uses in this area. The General Plan states that the Northwest Area should be developed with high density residential housing, commercial uses, offices, industrial uses, and parks. This Specific Plan is the culmination of a community process that looked at the Northwest Area at a level of detail greater than that of the General Plan to determine precisely what uses are most appropriate within the Northwest Specific Plan Area. In doing so, the Northwest Specific Plan Area process provided the community with an excellent opportunity to decide what land uses are most appropriate for this area and how new development in this area should contribute positively to the quality of life in Rohnert Park.

The Golf Course Drive West Improvements Project, recently complete, widened the existing roadway previously called Wilfred Avenue between Stony Point Road and Dowdell Avenue and Golf Course Drive West between Dowdell Avenue and Redwood Drive. The City is working with the County to change the name of the entire length of the roadway to Golf Course Drive West, which corresponds to the name of the current roadway segments inside Rohnert Park's city limits; therefore, for consistency the portion of Wilfred Avenue that bisects the Specific Plan Area will be referred to as Golf Course Drive West throughout this Specific Plan.

HOW TO USE THIS SECTION

This chapter of the Specific Plan provides a simple introduction and background to acquaint the reader with the Specific Plan Area and the purpose of the Specific Plan. The reader should read this section in order to gain a firm background from which to review further chapters.

PURPOSE OF THE SPECIFIC PLAN

This Specific Plan has been prepared consistent with the City of Rohnert Park's Municipal Code Chapter 17.06, Article VIII, Sections 17.06.290-450, SP-Specific Plan District. This Specific Plan is intended to serve as the primary document and reference guide for the future development of the Northwest Specific Plan Area for an expected timeline of about 20 years. In addition to providing the community and decision-makers with clear documentation of the vision for the Specific Plan Area, this Specific Plan is intended to provide a clear policy and regulatory framework by which future development projects and public improvements will be reviewed. Finally, this Specific Plan provides guidance on design, potential public investments, and implementation. It should be noted that a Specific Plan is not a detailed site plan or design plan and does not commit to any specific building design on any specific properties. This Plan is intended to provide a certain amount of flexibility to property owners and developers to allow for market-oriented solutions.

REGIONAL AND LOCAL SETTING

As shown on Figure 1-1, Rohnert Park is located in Sonoma County along Highway 101, north of San Francisco. Rohnert Park shares a border with the City of Cotati and is located north of the City of Petaluma and south of the City of Santa Rosa.

SPECIFIC PLAN AREA

As shown in Figure 1-2, the Northwest Specific Plan is located in unincorporated Sonoma County just outside the northwest city limits and west of Highway 101. The Specific Plan Area is approximately 100 acres in area (inclusive of existing roadways) and is bounded by Dowdell Avenue to the east, and stretches as far south as Business Park Drive to the south and as far north as Millbrae Avenue to the north. The westerly boundaries vary depending on the location, but go no further than Langner Avenue. The Plan Area is largely undeveloped with the exception of some small rural single-family homes. For more detail about the character of the Specific Plan Area, please refer to Chapter 2 of this Specific Plan. Figure 1-3 shows the Plan Area within the context of the city.

USE OF THE SPECIFIC PLAN

After adoption by the Rohnert Park City Council, this Specific Plan will be a public document that establishes the amount, type, and location of urban development that will be permitted in the Northwest Specific Plan Area. The Specific Plan also provides development standards that regulate the built form of new development concerning height, building setbacks, parking requirements, and other similar development features. Additional requirements for development may be required under the regulations in the Municipal Code beyond this Specific Plan.

Figure 1-1: REGIONAL CONTEXT

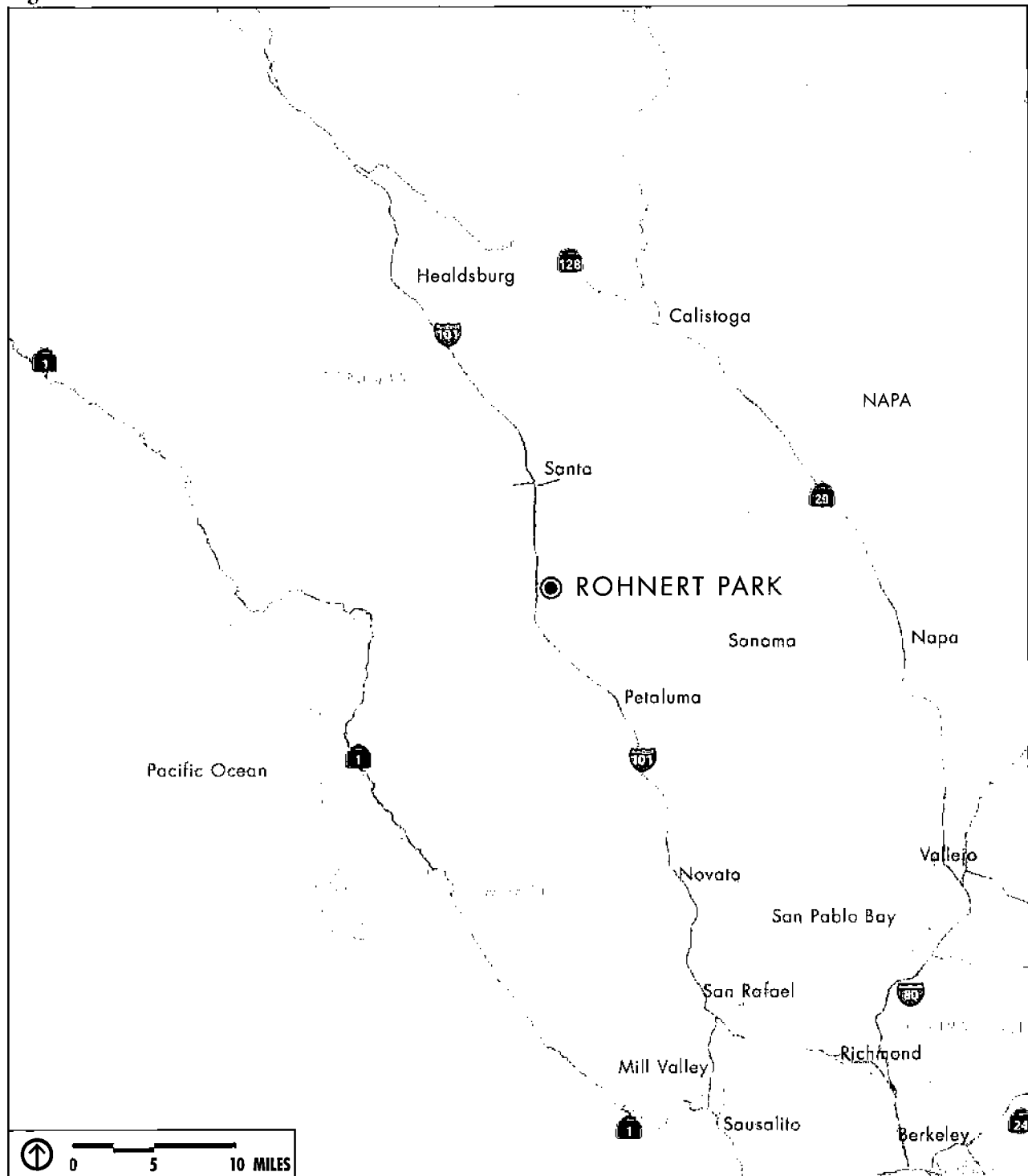


Figure 1-2: **SPECIFIC PLAN AREA**

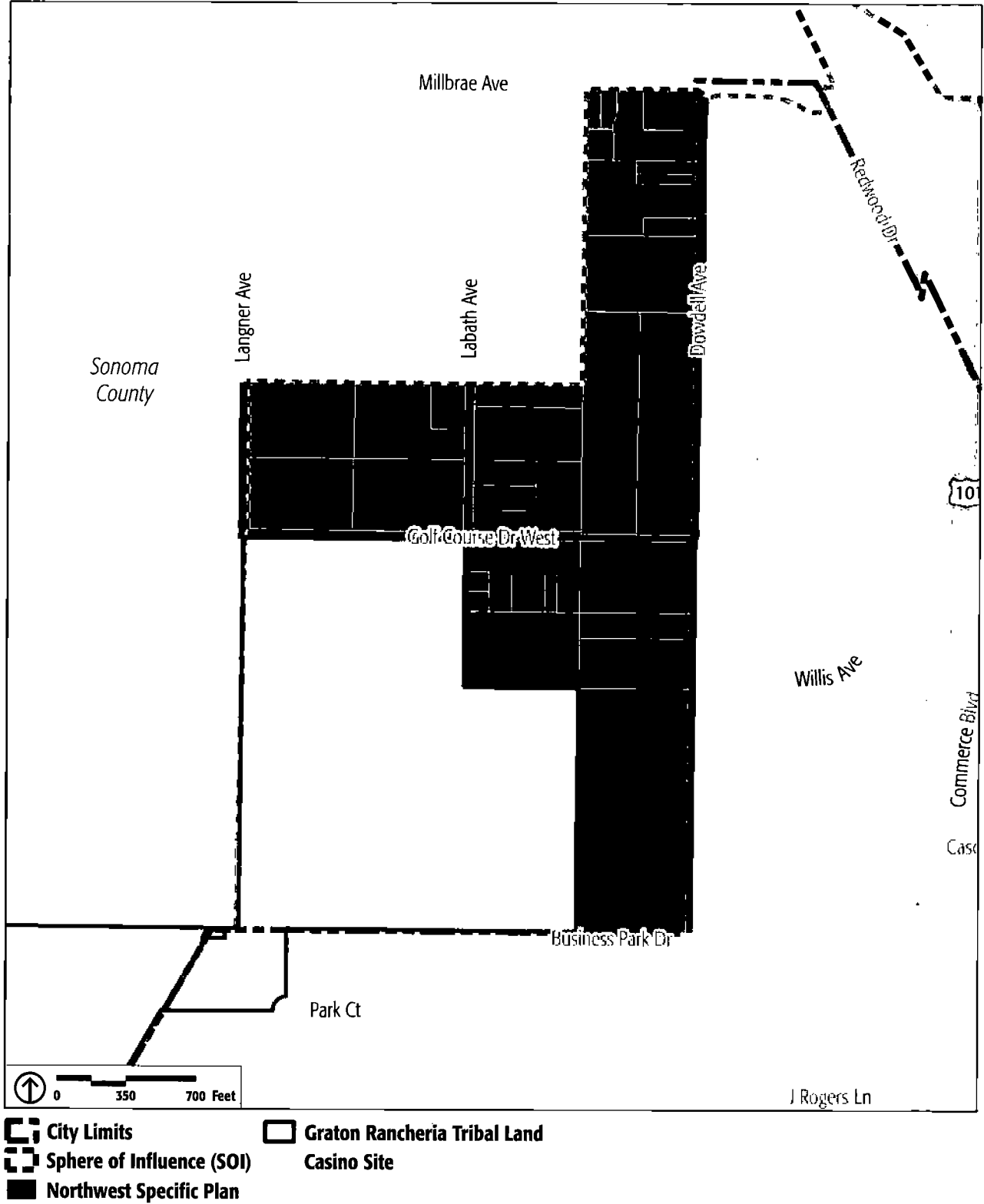
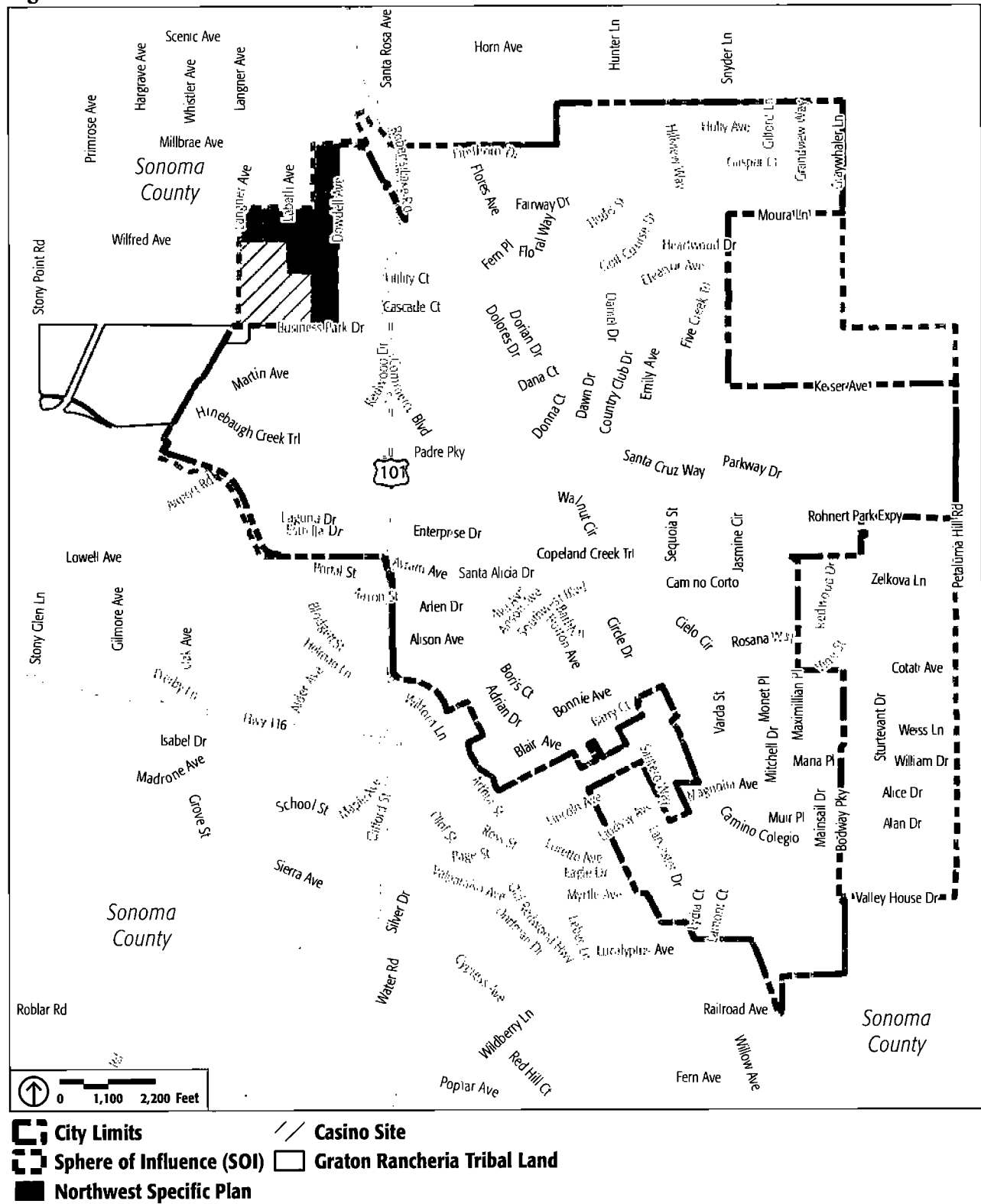


Figure 1-3: CITY CONTEXT



All future development projects within the Northwest Specific Plan Area will be subject to review for consistency with the policies, standards, and requirements of this Specific Plan. If consistent with the Specific Plan, it is anticipated that future development will have already been assessed under the programmatic environmental documentation prepared for this Specific Plan. However, each new development may need to conduct additional review depending on the site-specific details associated with the particular development.

STATUTORY REQUIREMENTS FOR THE SPECIFIC PLAN

This Specific Plan has been prepared in accordance with the requirements of California Government Code Section 65451. As prescribed by law, the Plan includes text and diagrams that generally describe the following:

- The distribution, location and extent of all land uses, including open space.
- The proposed distribution, location, extent, and intensity of major components of public infrastructure, such as transportation and drainage systems.
- The standards and criteria by which development will proceed.
- A program of implementation measures, such as financing measures, policies, regulations, and public works projects.
- A statement of the relationship of the Specific Plan to the General Plan.

Chapter 17.06, Article VIII of the City's Zoning Code provides several specific requirements for Specific Plans, which have been addressed in this Specific Plan to the extent feasible. Many of the zoning code requirements Chapter 17.06, Article VIII will need to be addressed on the project level when more detail about the specific details of new development are known.

RELATIONSHIP TO GENERAL PLAN

The City of Rohnert Park's General Plan provides a comprehensive statement of the objectives, themes, and policies which the community is seeking to achieve in the areas of land use, growth management, community design, transportation, open space, parks and public facilities, environmental conservation, health and safety, noise, and housing. This Specific Plan is one tool the City uses to implement the General Plan.

A General Plan amendment will be required as part of the of the Specific Plan approval. The recommendations in this Specific Plan are consistent with the broad goals of the General Plan. The General Plan's policies call for a mix of industrial, office, multi-family residential, commercial, and mixed uses in this area, as well as supporting open spaces. The goals, policies, and standards contained in this Specific Plan further these goals.

PLAN CONTENTS

The chapters described below follow this introduction.

- **Chapter Two: Plan Area Characteristics** provides an overview of the existing setting of the Northwest Specific Plan Area, including a description of current land uses, circulation, infrastructure, and governing jurisdictions.
- **Chapter Three: Northwest Specific Plan Area Vision** provides a narrative description of the community-generated vision for the Northwest Specific Plan Area, as well as goals and policies that will guide implementation of the Specific Plan over the next 20 years.
- **Chapter Four: Land Use** provides the Land Use Designations that will apply to parcels in the Specific Plan Area, as well as a description of the generally intended uses, allowable land use intensities, and residential densities allowed within each Land Use Designation.
- **Chapter Five: Permitted Land Uses and Development Standards** provides a table of permitted land uses applicable to the Plan Area and specific standards for the built form and site planning components of new development, including features such as building heights, floor area ratios, setbacks, open space requirements, and other similar features.
- **Chapter Six: Circulation** provides recommendations for all modes of travel. Focuses on pedestrians, bicycles, and vehicular improvements that will be required to meet the needs of development called for in the Specific Plan.
- **Chapter Seven: Utilities and Infrastructure** provides recommendations for stormwater, wastewater, and water infrastructure improvements.
- **Chapter Eight: Implementation and Financing Plan** discuss potential funding opportunities and financing strategies for improvements recommended in this Specific Plan.

Chapter Two

PLAN AREA CHARACTERISTICS

This chapter provides a brief overview of the Plan Area as it existed during the writing of the Northwest Specific Plan. For the purposes of this chapter and this document, the Plan Area is referred to in two sub-areas: north district and south district. The north district includes all Plan Area properties north of Golf Course Drive West and the south district includes all Plan Area properties south of Golf Course Drive West.

HOW TO USE THIS SECTION

This chapter of the Specific Plan is intended to acquaint the reader with the Specific Plan Area as it is today. The reader should use this chapter to understand the current character and constraints for the Specific Plan Area.

PLANNING CONTEXT

The immediate planning context of the Northwest Specific Plan includes a number of significant and relevant projects within and near to the Plan Area. These are described below.

GRATON RANCHERIA RESORT AND CASINO

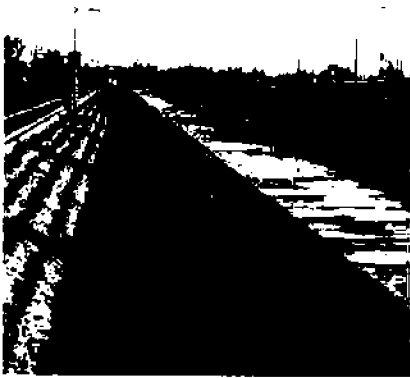
The Graton Rancheria Resort and Casino opened for operation on November 5, 2013 just west of the Northwest Specific Plan Area's south district. The resort and casino are bounded to the west by Langner Avenue, to the south by Business Park Drive, and to the north by Golf Course Drive West. The project includes a full-service casino, lobby and bar, hotel (future phase), and a pool/spa area (future phase).

WILFRED/DOWDELL VILLAGE SPECIFIC PLAN

The Wilfred/Dowdell Village Specific Plan is a long-term plan adopted by the City of Rohnert Park for the area immediately east of the Northwest Specific Plan Area's south district. The Wilfred/Dowdell Plan Area includes one large parcel north of Golf Course Drive West and spans south of Golf Course Drive

Northwest Specific Plan November 2014

West toward Business Park Drive, but does not include the last parcel that is immediately adjacent to Business Park Drive. The Plan calls for retail development just north of Golf Course Drive West as well as south of Golf Course Drive West. The portion of the Wilfred/Dowdell Plan Area north of Golf Course Drive West has an approved project, which includes a hotel and fast food service establishment. This project is not yet under construction.



GOLF COURSE DRIVE WEST IMPROVEMENTS PROJECT

The Golf Course Drive West Improvements Project was recently completed from Redwood Drive to Stony Point and the improved roadway is open and operating. Previously called Wilfred Avenue, the road is now referred to as Golf Course Drive West. The Golf Course Drive West Improvements project spanned approximately 1.79 miles, along what was the old Wilfred Avenue and now currently Golf Course Drive West, from Stony Point Road to Redwood Drive and approximately 0.14 miles of the road's length is within city limits. Approximately 1.65 miles are currently within Sonoma County. Golf Course Drive West bisects the Specific Plan

Area between Dowdell Avenue and Langner Avenue, and the Wilfred/Dowdell Village Specific Plan Area between Redwood Drive and Dowdell Avenue.

Upon agreement with Sonoma County, the entire length of the roadway will be renamed to Golf Course Drive West, which corresponds to the name of the current roadway segments inside Rohnert Park's city limits and west of Highway 101.

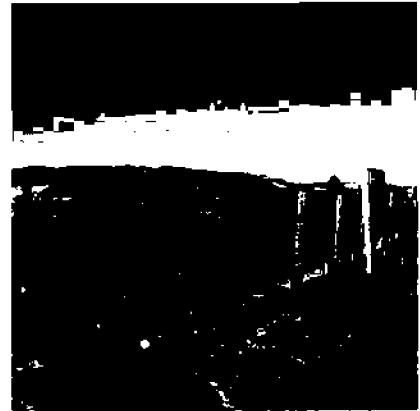
The Golf Course Drive West Improvements generally widened Golf Course Drive West from Stony Point Road to Redwood Drive. The improvements also included the construction of a new pedestrian/bicycle bridge over Bellevue-Wilfred Channel, three new traffic signals, the modification of an existing traffic signal at Redwood Drive, construction of curbs and sidewalks for pedestrians for portions of the street, undergrounding overhead utilities in some locations, relocating overhead utilities in some locations, installing new lighting, and constructing additional infrastructure improvements to ensure adequate drainage. The Golf Course Drive West Improvements required the excavation of the existing roadway and swales, and the clearing of land needed to supply adequate right-of-way for the widening.

EXISTING PLAN AREA LAND USES

The Plan Area has a limited number of existing developed land uses, as shown in Figure 2-1. This section describes the land uses that were found in the Plan Area based on field reconnaissance completed in May 2012.

AGRICULTURE/GRAZING

Agricultural land uses within the Plan Area are concentrated in the south district. This land use category includes farming, including small acreage intensive farming and part time farming activities. Lot size varies from three to fifteen acres and no residential uses are present. Primary activity appears to be grazing.



RURAL RESIDENTIAL

Single Family Rural residential uses within the Plan Area are concentrated in the north district. Lot size varies from one to seven acres. The primary use is detached single family homes with some agricultural activity such as grazing.



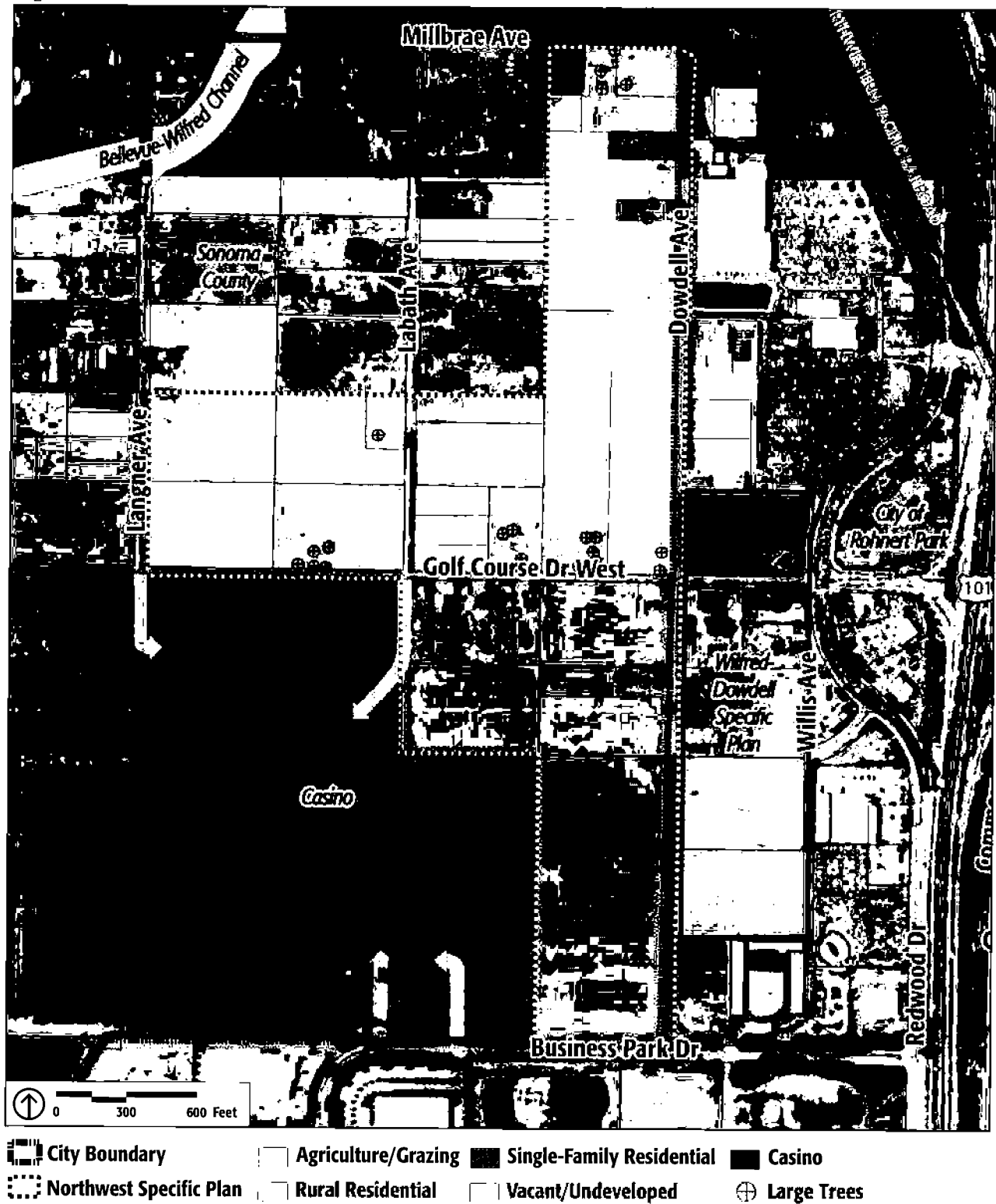
SINGLE FAMILY RESIDENTIAL

Single-family residential uses within the Plan Area are scattered in the south district, east of Labath Avenue and near the Dowdell Avenue/Millbrae Avenue Intersection. Lot sizes are generally an acre or less.

VACANT/UNDEVELOPED

Vacant land includes properties that are undeveloped and contain no usable structures. These lands may have been previously agricultural. Within the Plan Area, vacant/undeveloped land exists just north of Golf Course Drive West between Dowdell and Langner Avenue.

Figure 2-1: EXISTING LAND USE AND CHARACTER



SURROUNDING LAND USES

CASINO

A full service casino with supportive retail and entertainment uses is provided within the Graton Rancheria Casino project immediately adjacent west of the Plan Area's south district. Although not technically within the Plan Area, this important and critical use is shown in Figure 2-1 to show its relationship relative to the Northwest Specific Plan Area.



SINGLE-FAMILY RURAL RESIDENTIAL

Single-family rural residential uses near the Plan Area are located directly to the north and to the northwest of the Plan Area. These are mainly low density residential development on lands that are served by County maintained roads. The primary use in these areas is single family homes.

SINGLE-FAMILY RESIDENTIAL

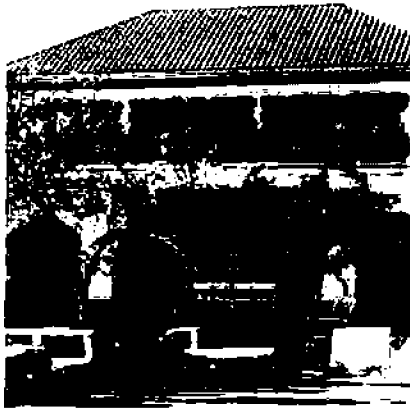
Single-family residential uses near the Plan Area are concentrated mainly to the southwest bordered by Rohnert Park Expressway and the Redwood Creek Apartments to the east. The single family residential development is called Rancho Verde. There is more single-family residential to the east of the Plan Area across the Highway 101 next to the golf course.

MULTI-FAMILY RESIDENTIAL

Multi-family residential uses include apartment buildings, duplexes, townhomes and condominiums. Multi-family use exists as an isolated apartment complex to the southwest of the Plan Area at Rohnert Park Expressway and Labath Avenue. The complex is called Redwood Creek.

AGRICULTURAL

Agricultural land uses near the Plan Area are concentrated to the south of Golf Course Drive West. This land use category includes farming, including small acreage intensive farming and part time farming activities. Lot size varies from six to seventy acres. The most common activity appears to be grazing.



LARGE FORMAT RETAIL

Large format retail uses surrounding the Plan Area include a Home Depot, Walmart, Costco, Linens n Things, PepBoys and Dollar Tree. These are located directly east of the Plan Area, adjacent to Highway 101 and fronting Redwood Drive. Costco is located further south of the Plan Area, along Redwood Drive.

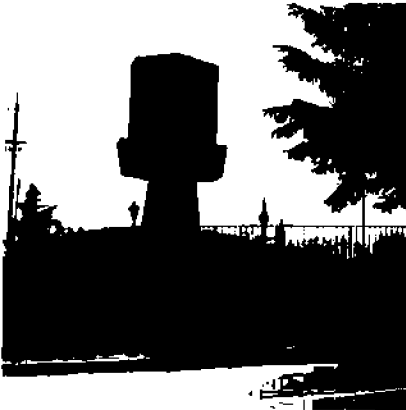
RETAIL

Retail uses surrounding the Plan Area include restaurants, fast food chains, beauty supply stores, clothing stores and other similar uses. Additional retail uses are located to the southwest of the Plan Area inside of the Rohnert Park Business Park Industrial area described below. Another center is to the southeast of the Plan Area on the east side of Highway 101. More retail uses exist south of Rohnert Park Expressway.



INDUSTRIAL

Industrial uses include auto repair, storage facilities, research and development, manufacturing, warehouses and other similar uses. Industrial uses are mainly isolated to the southeast of the Plan Area adjacent to the east side of Highway 101 and within the Rohnert Park Business Park.



COMMERCIAL AND ENTERTAINMENT

Commercial uses include hotels, Scandia Fun Center, golf course and other uses ranging from department stores and specialty shops to space extensive businesses such as paint, tire, carpet, lumber, home materials, and feed stores. These uses exist mainly along the west side of Highway 101 from Golf Course Drive West to the Rohnert Park Expressway. The Scandia Fun Center is located directly east of the Plan Area on the west side of Highway 101. The golf course is located on the east side of Highway 101, directly east of the Plan Area.

OFFICE

Office uses are mainly located directly to the south of the Plan Area within Rohnert Park Business Park adjacent to the south side of Business Park Drive. Additional uses are located just east of this business park adjacent to the west side of Highway 101.

VACANT/UNDEVELOPED

Vacant land includes properties that are undeveloped or contain vacant structures. These uses are isolated to the south of the Plan Area, directly south of agricultural areas.

PUBLIC

Public uses include government buildings, schools, hospitals and other social services. Public uses near the Plan Area are government owned. These uses include the Highway Patrol and City of Rohnert Park Administrative buildings. Highway Patrol is located just north of the Rohnert Park Expressway adjacent to Labath Avenue. The City of Rohnert Park buildings are located southeast of the Plan Area at J Rogers Lane.

CIRCULATION NETWORK

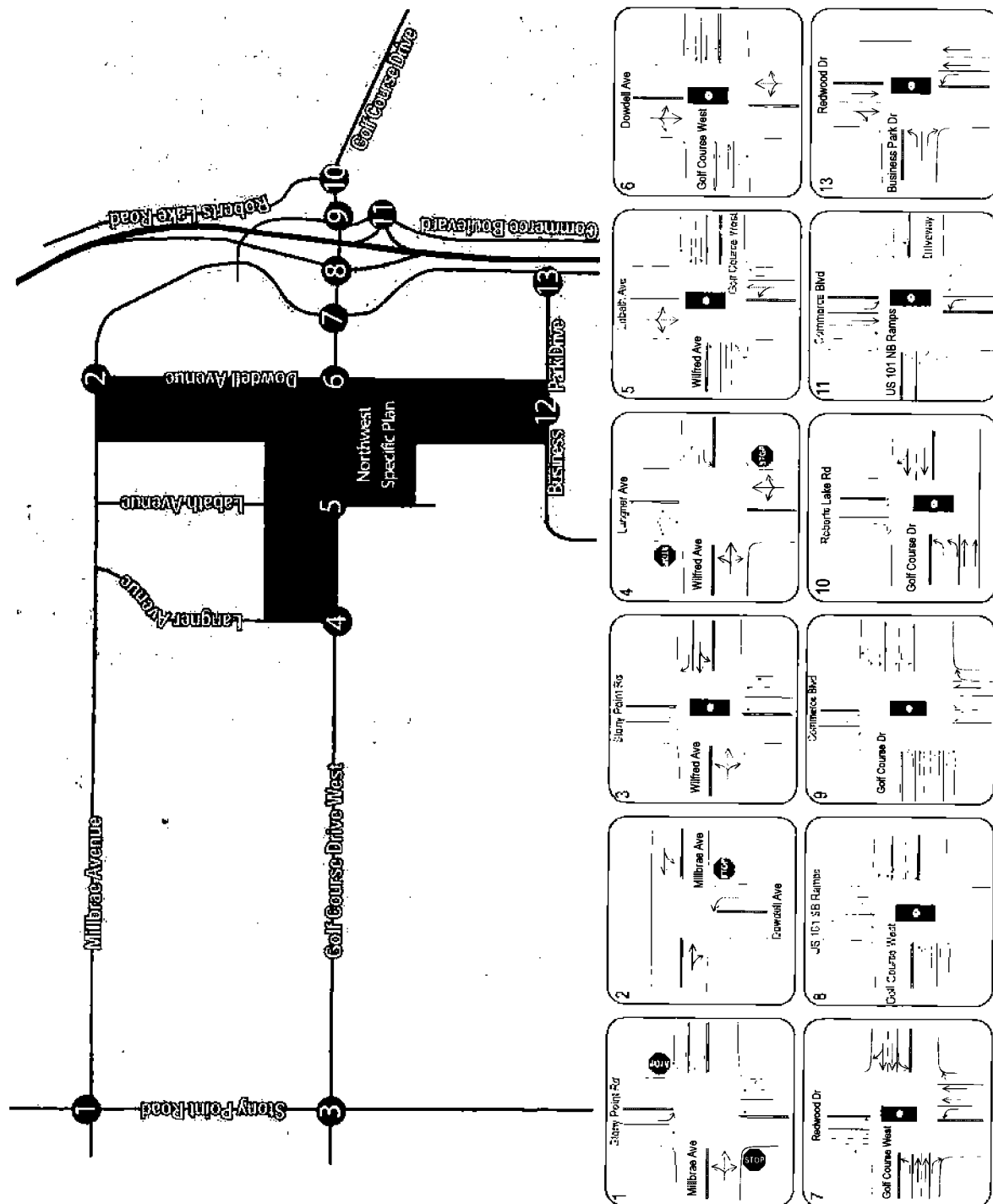
The Specific Plan Area is currently rural in nature. With the exception of Golf Course Drive West, which was widened in 2013, many of the roadways that traverse the area are typically two-lane streets with minimal to no shoulders, and surface drainage ditches on each side. Approximately one-quarter to one-half mile to the east are a mix of transportation resources that provide local and regional access, including Highway 101 and the Golf Course Drive West interchange, multi-use pathways, bike lanes, sidewalks, the Caltrans Roberts Lake Road Park-and-Ride lot which provides 180 parking spaces, and bus transit lines. The local circulation system serving the Plan Area, including the intersections that were studied as part of the Specific Plan process, is shown in Figure 2-2. More detail on the existing and future operations at these intersections can be found in the Northwest Specific Plan Draft Environmental Impact Report (EIR).

1. Stony Point Road/Millbrae Avenue (County of Sonoma)
2. Millbrae Avenue/Dowdell Avenue
3. Stony Point Road/Wilfred Avenue (County of Sonoma)
4. Golf Course Drive West¹/Langner Avenue (County of Sonoma)
5. Golf Course Drive West/Labath Avenue
6. Golf Course Drive West/Dowdell Avenue
7. Golf Course Drive West/Redwood Drive
8. Golf Course Drive West/U.S. 101 South Ramps
9. Golf Course Drive/Commerce Boulevard
10. Golf Course Drive/Roberts Lake Road
11. Commerce Boulevard/U.S. 101 North Ramps
12. Business Park Drive/Dowdell Avenue²
13. Business Park Drive/Redwood Drive

¹ Was Wilfred Avenue prior to the renaming of Golf Course Drive West in the summer of 2013.

² Future intersection

Figure 2-2: LOCAL CIRCULATION SYSTEM



Source: Whitlock & Weinberger Transportation, Inc. (W-Trans), 2013.

Following is a description of the key roadways within and surrounding the Plan Area.

HIGHWAY 101

Highway 101 is the primary route connecting the City of Rohnert Park to the San Francisco Bay Area to the south and Santa Rosa to the north. Within Rohnert Park, Highway 101 is a six-lane freeway with auxiliary lanes connecting on- and off-ramps at the Golf Course Drive and Rohnert Park Expressway interchanges. These interchanges provide convenient access to the Plan Area.

REDWOOD DRIVE

Redwood Drive is identified as a major arterial in the Rohnert Park General Plan. The corridor extends from SR 116 in Cotati to Millbrae Avenue. Redwood Drive includes four travel lanes, planted medians and/or two-way left turn lanes, bike lanes, and sidewalks in the Plan Area vicinity. The posted speed limit within Rohnert Park is 40 mph, except for the segment between Commerce Boulevard and Willis Road, which is currently posted at 35 mph.

COMMERCE BOULEVARD

Commerce Boulevard is identified as a major arterial in the Rohnert Park General Plan, and extends from SR 116 in Cotati to just north of Golf Course Drive West, where it turns west and crosses under Highway 101 and connects to Redwood Drive. Commerce Boulevard has posted speed limits of 35 and 40 mph.

MILLBRAE AVENUE

Millbrae Avenue is located primarily outside of the Rohnert Park city limits. The street is identified as a rural minor collector in the Sonoma County 2020 General Plan. The roadway has a paved width ranging from 20 to 24 feet, and serves rural residential uses between the City of Rohnert Park and Stony Point Road. The roadway is flanked by deep drainage swales and includes no curb, gutter, sidewalk, or bicycle facilities. The posted speed limit is 40 mph.

LANGNER AVENUE AND LABATH AVENUE

Within the Plan Area boundaries, both Langner Avenue and Labath Avenue are currently located outside of the Rohnert Park city limits. They are rural residential streets connecting rural residences to Millbrae Avenue and Golf Course Drive West. To the north of Golf Course Drive West, the two roadways have paved widths ranging from 14 to 18 feet, with a small section of Langner Avenue near Millbrae Avenue at approximately 20 to 24 feet wide. Like Millbrae Avenue, both roadways are flanked by deep drainage swales and include no curb, gutter, sidewalk, or bicycle facilities. The posted speed limit on Langner Avenue is 30 mph.

DOWDELL AVENUE

Dowdell Avenue has a 40-foot paved width with sidewalk on the east side of the street between Millbrae Avenue and approximately 375 feet north of Golf Course Drive West. Approaching Golf Course Drive West, Dowdell Avenue narrows to a configuration similar to Millbrae Avenue on the continuing segments to the south. The two-lane street is designated as a two-lane minor collector in the Rohnert Park General Plan.

STONY POINT ROAD

Stony Point Road is a north-south rural arterial running through Sonoma County to the west of Rohnert Park. The corridor connects Santa Rosa to Petaluma and outside of these urban areas is largely configured as a two-lane highway. In the vicinity of Rohnert Park the roadway's posted speed limit is 50 mph.

GOLF COURSE DRIVE

Golf Course Drive is an east-west secondary arterial that connects the northeastern portions of Rohnert Park to Highway 101. On the east side of Highway 101, the corridor includes four lanes, on-street bicycle lanes, and sidewalks on both sides of the street except along the golf course, where the street has two lanes, on-street bicycle lanes, and a multi-use path on the north side of the street. Recently, Golf Course Drive was extended to Golf Course Drive West (Wilfred Avenue) on the west side of Highway 101 via a freeway underpass. The posted speed limit is 35 mph.

GOLF COURSE DRIVE WEST

Golf Course Drive West runs between Highway 101 and the western Rohnert Park city limits, and to the west of the city limits it was called Wilfred Avenue before being renamed Golf Course Drive West. The street includes two through lanes in each direction between Redwood Drive and Labath Avenue, two eastbound lanes and one westbound lane between Labath Avenue and Langner Avenue, and one lane in each direction between Langner Avenue and Stony Point Road. The entire length of the roadway includes on-street bicycle lanes. The segment between Langner Avenue and Redwood Drive includes sidewalks on the south side of the streets, as well as medians and left turn pockets at several locations. The posted speed limit is 40 mph.

BUSINESS PARK DRIVE

Business Park Drive is identified as an east-west minor collector in the Rohnert Park General Plan and connects nearby commercial neighborhoods to major arterials such as Redwood Drive. The street consists of two travel lanes with continuous sidewalks on the south side of the street. The posted speed limit is 40 mph.

PEDESTRIAN AND BICYCLE NETWORK

With the exception of on-street bicycle lanes on Golf Course Drive West, as well as sidewalks along the south side of the Golf Course Drive West corridor, there are currently no pedestrian or bicycle facilities present within the boundaries of the Northwest Specific Plan. A developed network of pedestrian and bicycle facilities does, however, exist one-quarter to one-half mile to the east. A description of these facilities is provided below.

Continuous sidewalks exist on the west side of Redwood Drive where the street abuts Highway 101 or the Sonoma-Marin Area Rail Transit (SMART) rail corridor, and on both sides of the street elsewhere. Continuous sidewalks exist along the east side of Dowdell Avenue from Redwood Drive to the southern Home Depot driveway, and along the south side of Business Park Drive. The recently completed Golf Course Drive underpass at Highway 101 includes sidewalks on both sides of the street. An eight-foot wide Class I multi-use pathway is also located on the south side of Commerce Boulevard under Highway 101. Marked crosswalks are provided on the south leg of the Redwood Drive/Golf Course Drive West intersection and on the west leg of the Redwood Drive/Business Park Drive intersection, but are otherwise not present at any of the study intersections near the Plan Area west of Highway 101. Class II on-street bicycle lanes are provided along the entire length of Redwood Drive within Rohnert Park, as well as along Golf Course Drive-Golf Course Drive West between Stony Point Road and the eastern city limits.

Beyond the Specific Plan Area, Rohnert Park has a well-developed pedestrian network with sidewalks provided on both sides of most streets. Rohnert Park also has over seven miles of Class I multi-use pathways throughout the city, most of which exist along creeks. A major off-street pathway running through the entire City is also planned to be constructed along the SMART rail corridor. The SMART pedestrian-bicycle network would ultimately extend the length of the rail line from Cloverdale to Larkspur using a combination of off-street pathways and on-street bicycle lanes. Near the vicinity of the Plan area, the SMART multi-use pathway would run along the east side of the railroad tracks, and accessible at the Golf Course Drive railroad crossing. SMART estimates that the segment of the pathway between Golf Course Drive and Todd Road may carry approximately 340 bicyclists per day.

The Sonoma County Transportation Authority (SCTA) Countywide Bicycle & Pedestrian Master Plan, May 2008, designates existing and future bicycle and pedestrian facilities within Rohnert Park. The Plan includes both policy and physical project recommendations. Physical projects identified in the plan near the Northwest Specific Plan include the proposed Class I trail along Bellevue Creek; proposed Class II bike lanes on Stony Point Road, Millbrae Avenue, and Dowdell Avenue; as well as increased bicycle parking at the Roberts Lake Transit Park and Ride. The bicycle plan also depicts the SMART multi-use path as well as a future multi-use path along the Bellevue channel to the north of the Specific Plan area that will intersect the SMART path.

TRANSIT FACILITIES

SONOMA COUNTY TRANSIT

Sonoma County Transit (SCT) is the principal transit service within Rohnert Park, providing daily local and intercity service. SCT local Routes 10, 12, and 14 operate together to provide transit access to destinations on both the east and west sides of Highway 101. Each local route operates with approximately 90- to 120-minute headways between 6:00 a.m. and 6:00 p.m. on weekdays, and 9:30 a.m. and 3:00 p.m. on Saturdays; no local service is provided on Sundays.

SCT Routes 44 and 48, with service between Petaluma and Santa Rosa, provide intercity service to Rohnert Park. These routes, which generally run along the same streets north of Cotati, operate with approximately 40 to 120 minute headways between 6:30 a.m. and 8:30 p.m. on weekdays, with a combined headway of 30 to 60 minutes. On weekends, SCT Routes 44 and 48 operate with approximately two to four hour headways between 7:00 a.m. and 8:00 p.m. and a combined headway of one to two hours.

The nearest SCT bus stops serving the Plan Area are located in the Walmart shopping center parking lot, Redwood Avenue/Commerce Boulevard, Golf Course Drive/Highway 101 Southbound Ramps, and Redwood Drive between Golf Course Drive West and Business Park Drive. On weekdays, the Plan Area is served by SCT Routes 14 and 44, with SCT Route 48 stopping a half-mile walk away at Golf Course Drive/Robert Lakes Road. On weekends, the Plan Area is served by SCT Routes 12, 44, and 48.

All Sonoma County Transit buses are wheelchair lift-equipped and can transport two wheelchair passengers at a time. Sonoma County Transit allows bikes on all of its buses. Buses are equipped with a front-loading bike rack that accommodates either two or three bicycles. When the front-loading rack is full, bus drivers may allow up to two bikes inside the bus.

GOLDEN GATE TRANSIT

Golden Gate Transit (GGT) provides daily interregional service along the Highway 101 corridor between Santa Rosa and San Francisco. Route 72 provides weekday commuter service between Santa Rosa and San Francisco, with a southbound stop at Golf Course Drive West/Redwood Drive and both northbound and southbound stops at Golf Course Drive West/Robert Lakes Drive. Route 72 operates with 20- to 30-minute headways on weekdays only, with southbound service into San Francisco between 4:00 a.m. and 7:00 a.m. and northbound service from San Francisco between 3:30 p.m. and 7:30 p.m.. GGT Routes 80 and 101 operate daily along the Highway 101 corridor between Santa Rosa and San Francisco, with a stop on Commerce Boulevard at Rohnert Park Expressway. Route 80/101 buses have approximately one-hour headways in each direction between 4:00 a.m. and 10:00 p.m. on both weekdays and weekends. All GGT buses are handicap accessible and equipped with a front loading bike rack that accommodates either two or three bicycles. On express buses, storage space for bicycles is provided beneath the coach.

DIAL-A-RIDE

Dial-a-ride, also known as paratransit or door-to-door service, is available for those who are unable to independently use the transit system due to a physical or mental disability. Sonoma County Paratransit is designed to serve the needs of individuals with disabilities within Sonoma County. Service days are Monday through Friday from 5:00 a.m. to 11:00 p.m., and Saturday and Sunday from 7:00 a.m. to 9:00 p.m..

WATER INFRASTRUCTURE

In general, a water system is composed of three facets: supply/production/treatment, conveyance/storage, and transmission/distribution. Below is a discussion of each of these three facets as relates to this Plan. The discussion summarizes the existing facilities and also summarizes existing plans for improvements to these facilities. Water facilities are shown in Figure 2-3.

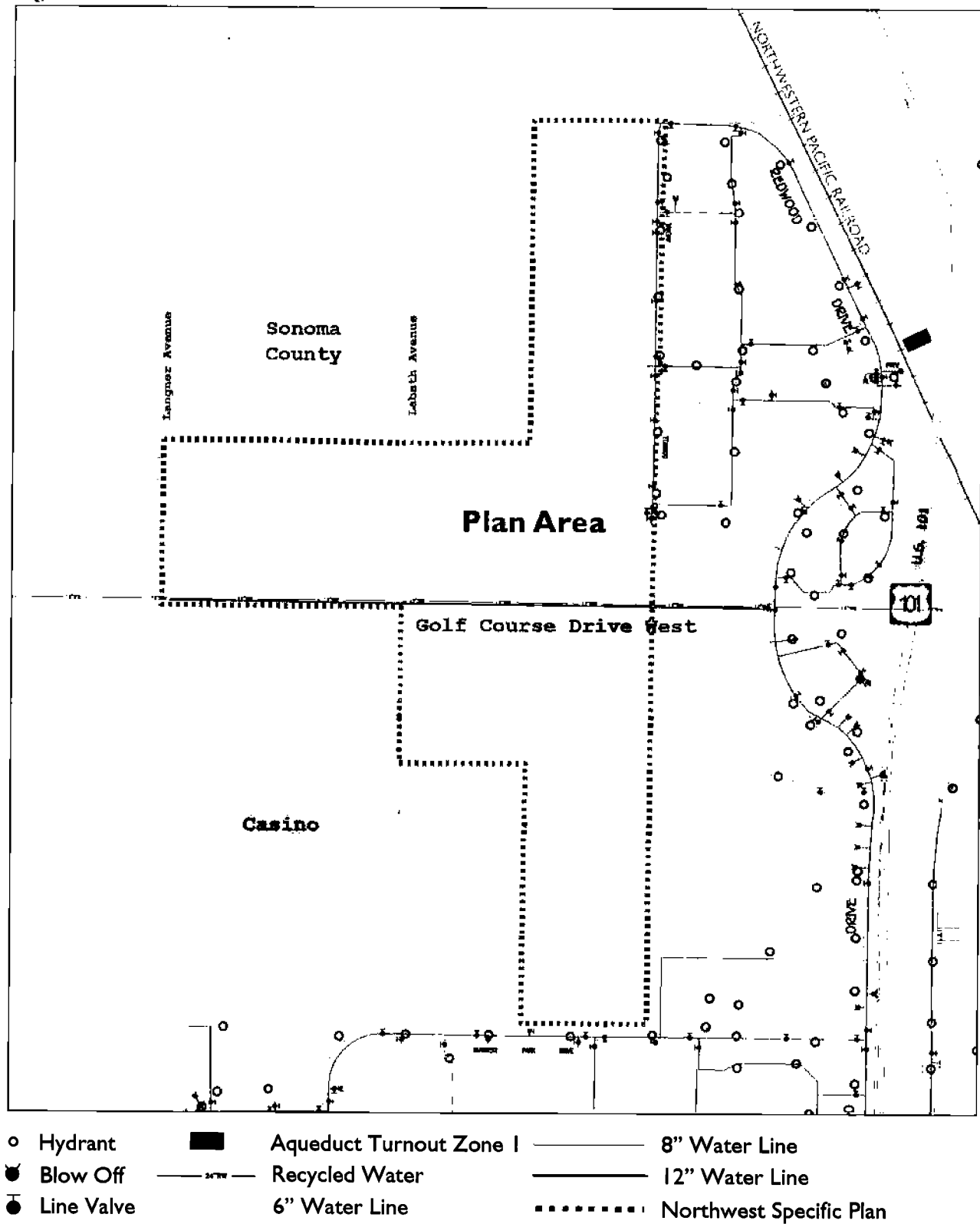
SUPPLY/PRODUCTION/TREATMENT

There are two sources of potable water for the City of Rohnert Park: City groundwater wells and Sonoma County Water Agency (SCWA) surface water. One well (W-41), located south of Business Park Drive, approximately halfway between Labath Avenue and Redwood Drive, is connected to the distribution system on the west side of Highway 101. Per Table 4.1 of the 2010 Urban Water Management Plan (UWMP, prepared by Winzler and Kelly, June 2011), the production rate for this well is 285 gallons per minute. There are no other wells (active, inactive, or standby) within one-half mile of the Plan Area. There is one Sonoma County Water Agency (SCWA) turnout (163), connected to the distribution system on the west side of Highway 101. Per Tables 3.15 and 4.11 of the UWMP, there is sufficient capacity for all undeveloped areas contemplated for growth through 2035, with diversions from SCWA increasing while groundwater pumping decreasing to meet these anticipated demands. According to the Water Model Study Summary Report (prepared by Brelje & Race, September 2004), piping improvements to SCWA turnout 163 will be needed to provide adequate water supply to the Plan Area.

CONVEYANCE/STORAGE

According to the Water Model Study Summary Report, the City has approximately 4.2 MG of storage. This does not reflect an additional 6.0 MGD that is assumed to be available based upon available water stored in SCWA tanks. The nearest City operated above-ground storage tank (Tank T-7) is located southwest of the Plan Area. According to the Water Model Study Summary Report, an additional 640,000 gallons of storage will be needed to provide adequate water storage for the Plan Area.

Figure 2-3: WATER INFRASTRUCTURE



DISTRIBUTION

There is an existing 12-inch water main in Golf Course Drive West in the Plan Area that stretches from Redwood Drive to Langner Avenue. There are existing 8-inch water mains located to the south of the Plan Area along Business Park Drive and east of the Plan Area along Dowdell Avenue and Redwood Drive, then northward on Dowdell to connect to the existing main behind the Home Depot. There are no water mains along the north or west side of the Plan Area. According to the Water Model Study Summary Report, a new 8-inch water main will need to be constructed along Redwood Drive, starting at the SCWA turnout and continuing south for 350 feet.

In addition to the potable water mains, a 12-inch recycled water main intersects the Plan Area along Golf Course Drive West, from Langner Avenue to Dowdell Avenue.

WASTEWATER INFRASTRUCTURE

In general, a wastewater system is composed of two facets: treatment/disposal and collection (mains and pump/lift stations). Below is a discussion of each of these facets as relates to this Specific Plan. The discussion summarizes the existing facilities and also summarizes existing plans for improvements to these facilities. Wastewater facilities are shown in Figure 2-4.

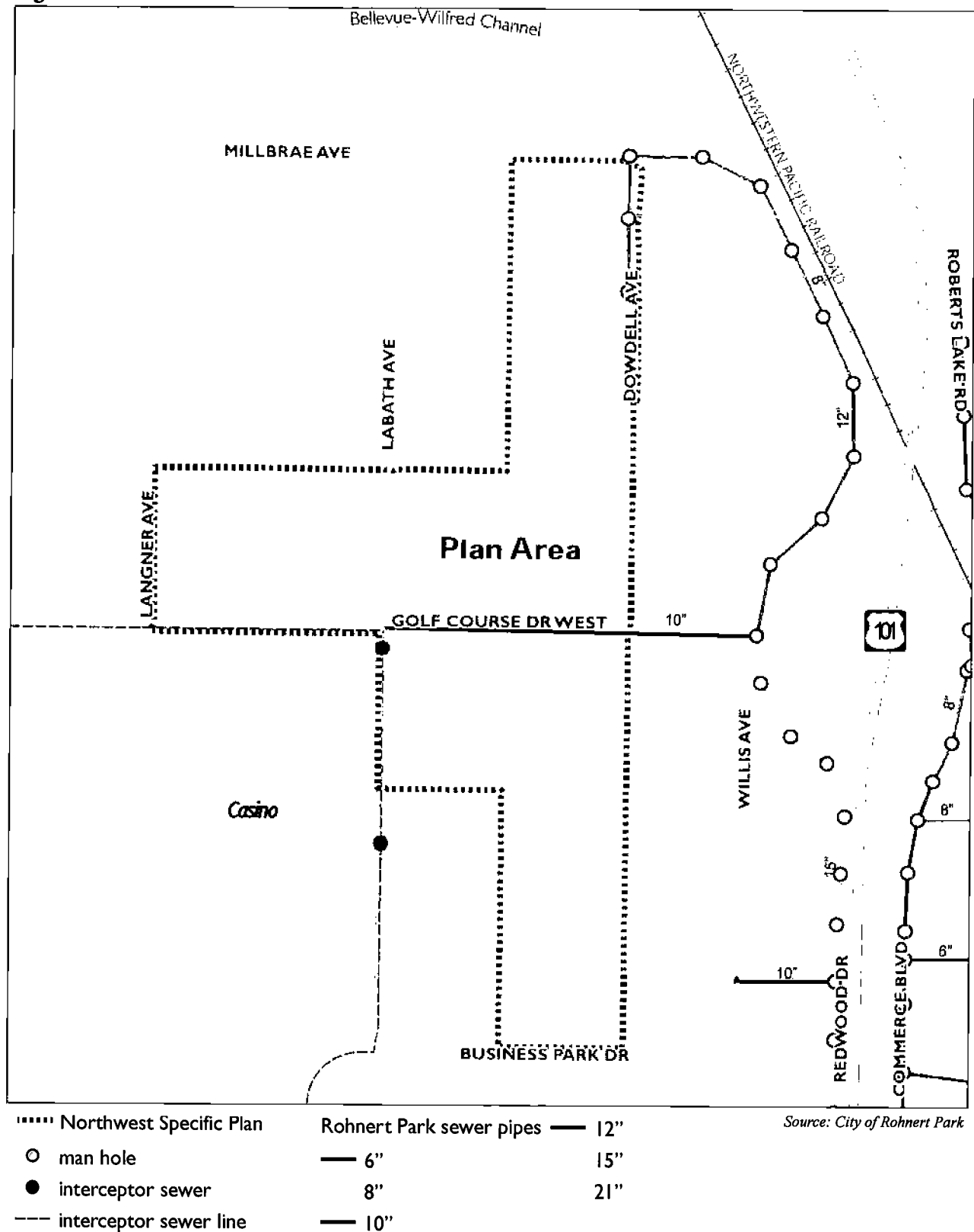
TREATMENT/DISPOSAL AND ALLOCATION CAPACITY

The City is one of five entities (City of Santa Rosa, City of Rohnert Park, City of Sebastopol, City of Cotati, and the South Park County Sanitation District) that comprise the Santa Rosa Subregional Sewer System. All wastewater treatment occurs at the Laguna Water Reclamation Plant located in Santa Rosa; the City of Rohnert Park does not treat any sewage.

The Fifth Amendment to the Agreement for Use of the Santa Rosa Subregional Sewerage System (executed November 19, 2008) identified the allocation of treatment capacity to all of the entities. The City's treatment allocation is 5.08 MGD average dry weather flow (ADWF) and disposal allocation as 4.15 mgd. For calendar year 2011, ADWF (period of time not influenced by rainfall) was estimated at 2.85+ MGD. According to City staff³, the City has adequate reserve in their allocation to accommodate wastewater from the Plan Area.

³ Conference call with City Engineer, Patrick Barnes, on July 5, 2012.
Northwest Specific Plan November 2014

Figure 2-4: WASTEWATER INFRASTRUCTURE



COLLECTION

There are existing 8-inch, 12-inch, and 15-inch sewer mains that serve the commercial areas to the east of the Plan Area. An 8-inch main exists along Dowdell Avenue (extends northerly approximately 600 linear feet to the intersection of Dowdell Avenue and Millbrae Avenue). This 8-inch main continues easterly along Millbrae Avenue, and then continues southerly along Redwood Drive. The sewer main size increases to 12 inches approximately 230 feet northerly of the intersection of Redwood Drive and Commerce Drive. The sewer main continues south, and the main size increases to 15 inches at the intersection of Golf Course Drive West and Redwood Drive. The 15-inch sewer main continues south to an existing lift station on J. Rogers Lane. In addition, there is an existing 10-inch sewer main along Golf Course Drive West from Redwood Drive to Labath Avenue.

There are no sewer mains along the north, south, or west side of the Plan Area. However, the casino development includes an 8-inch force main running eastwardly along Business Park Drive to the lift station on J. Rogers Lane. This force main is only available to serve the casino and not other properties within the Plan Area. Furthermore, no sewer mains are located within the Plan Area. The City's force main discharge to the City of Santa Rosa Laguna Treatment plant runs northwardly along Labath Avenue from Business Park Drive, thence westerly along Golf Course Drive West. According to City staff⁴, per the Public Facilities Finance Plan the existing lift station will need to be expanded and the costs will be covered by connection fees.

STORMWATER INFRASTRUCTURE

The Plan Area lies within the Laguna de Santa Rosa watershed, which encompasses an area of approximately 250 square miles. Runoff from the Laguna de Santa Rosa eventually drains into the Russian River and ultimately into the Pacific Ocean. The Laguna is the center of a significant floodplain. During peak storms, flooding along Laguna de Santa Rosa impacts the City of Rohnert Park and surrounding area. There are no surface water bodies or blue line streams (intermittent streams that flow for most or all of the year) within the Northwest Specific Plan Area.

REGIONAL DRAINAGE

Regionally, creeks flow from the eastern ridge of the Sonoma Mountains through drainage channels westward across Rohnert Park and discharge into Laguna de Santa Rosa. Currently, drainage within the City is conveyed through a storm drainage and flood control collection system consisting of underground pipes and natural and constructed channels. This system is under joint management of the City of Rohnert Park and the Sonoma County Water Agency (SCWA). The City maintains responsibility for the system of underground pipes, while the SCWA maintains the system of open channels that divert flow to Laguna de

⁴ Conference call with City Engineer, Patrick Barnes, on July 5, 2012.
Northwest Specific Plan November 2014

Santa Rosa. The City's storm drain system map is shown on Figure 2-5, and the SCWA's map of conveyance channels is shown on Figure 2-6.

LOCAL DRAINAGE

Storm drain infrastructure within the Plan Area includes culverts, surface drainage ditches, and canals that convey runoff and are typically located adjacent to public roadways. Bordering the Plan Area to the south is a man-made drainage channel (Labath Creek) along the north side of Business Park Drive. Beneath Business Park Drive are three 60-inch underground drainage pipes that convey runoff from the City to Hinebaugh Flood Control Channel. Along the east side of the Plan Area, there is an subsurface 15- to 42-inch storm drain along Dowdell Avenue in the north district that flows to the north. Additionally, as part of the recent Golf Course Drive West Improvement Project, bioretention swales and a 30- to 42-inch storm drain were recently installed along the south side of Golf Course Drive West that convey runoff to the west for eventual discharge into the Bellevue-Wilfred Channel. According to the Wilfred/Dowdell Village Specific Plan EIR, there presently is insufficient capacity in the Labath Creek Channel and Hinebaugh Flood Control Channel under 10-year storm drainage conditions.

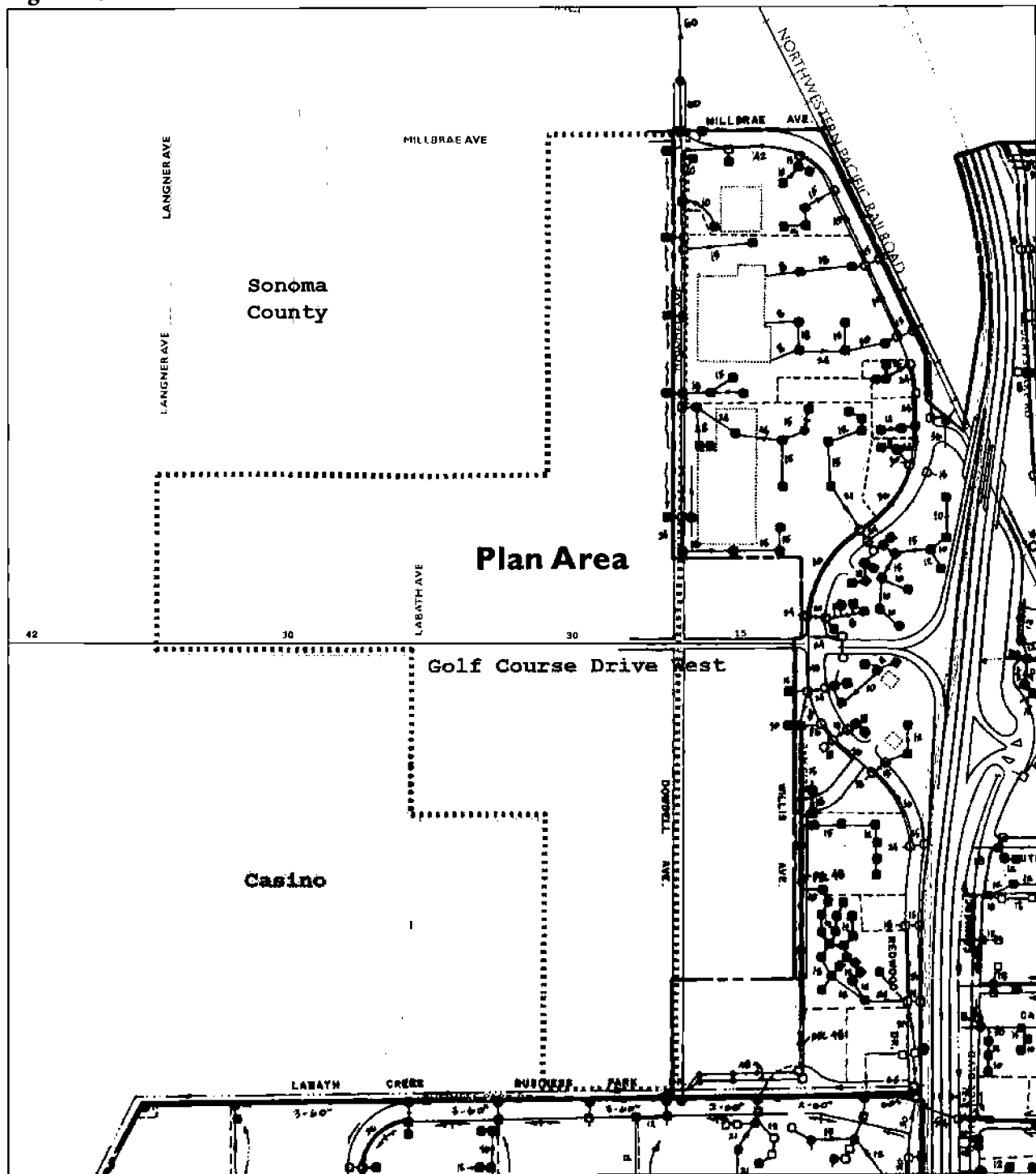
BIOLOGICAL CONDITIONS

This section discusses sensitive animal and plant habitat in the Plan Area.

POTENTIAL WETLANDS

Several potentially sensitive habitats which may be regulated under Section 404 (and 401) of the Clean Water Act were observed in the Plan Area. A routine protocol-level wetland delineation was conducted throughout some parcels of the Plan Area on May 6, 2013 to map the extent of these potentially sensitive habitats. This survey did not include all parcels due to property access issues. Several categories of wetlands, including seasonal wetland depressions, seasonal wetland meadows, and vernal pools, were observed on the surveyed sites. It is possible that similar conditions exist on sites that were not surveyed. No non-wetland waters were observed during the delineation site visit.

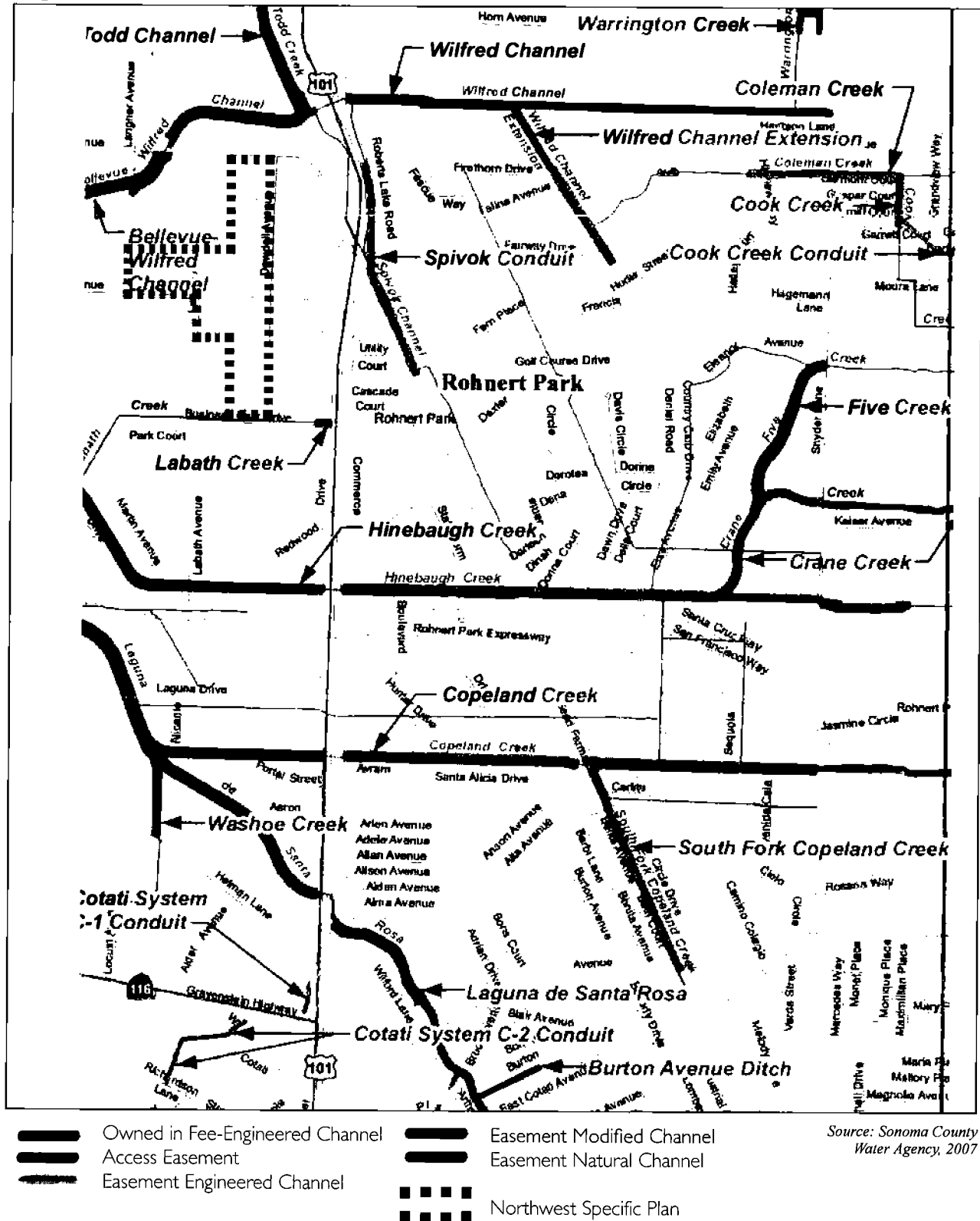
Figure 2-5: ROHNERT PARK STORM DRAINAGE SYSTEM



Source: Sonoma County, revised 11-96, PlaceWorks 2014

- ▣ Catch Basin
- Man Hole
- Northwest Specific Plan
- Drop Inlet
- ··· — Drainage Ditch

Figure 2-6: SONOMA COUNTY WATER AGENCY FACILITY GUIDE



SPECIAL STATUS PLANT SPECIES

Based on observed habitat and site conditions, the Plan Area has the potential to support 12 special-status plant species: Sonoma sunshine (*Blennosperma bakeri*), dwarf downingia (*Downingia pusilla*), hayfield tarplant (*Hemizonia congesta* ssp. *congesta*), Burke's goldfields (*Lasthenia burkei*), Contra Costa goldfields (*Lasthenia conjugens*), legenere (*Legenere limosa*), Sebastopol meadowfoam (*Limnanthes vincu-lans*), marsh microseris (*Microseris paludosa*), Baker's navarretia (*Navarretia leucocephala* ssp. *bakeri*), Lobb's buttercup (*Ranunculus lobbii*), showy rancheria clover (*Trifolium amoenum*), and Saline clover (*Trifolium hydrophilum*). Though no special-status plant species were observed during biological resources assessments, no determination regarding presence or absence can be made as no protocol-level rare plant surveys have been conducted within the Plan Area.

SPECIAL STATUS WILDLIFE SPECIES

Two special-status wildlife species were observed within the Plan Area, Northern harrier (*Circus cyaneus*) and Whitetailed kite (*Elanus leucurus*). In addition, based on the habitats and site conditions observed, six additional species have a moderate or high potential to occur within the Plan Area: hoary bat (*Lasiurus cinereus*), golden eagle (*Aquila chrysaetos*), loggerhead shrike (*Lanius ludovicianus*), grasshopper sparrow (*Ammodramus savannarum*), California tiger salamander (CTS, *Ambystoma californiense*), and vernal pool andrenid bee (*Andrena blennospermatis*). Similar to the special-status plant species discussed above, no determination of presence or absence can be made for the six wildlife species with potential to occur as no protocol-level surveys were conducted for these species.

The US Fish and Wildlife Service issued a final rule on Critical Habitat for the Sonoma County Distinct Population Segment (DPS) of CTS on August 31, 2011 (Federal Register 50 CFR Part 17; 76 FR 54346). The ruling designated approximately 47,383 acres as critical habitat and includes the entire Plan Area.

JURISDICTIONAL BOUNDARIES

The Northwest Specific Plan Area falls entirely within the jurisdictional boundaries of the County of Sonoma. The current Rohnert Park city limits meet the Plan Area's eastern boundary. The Plan Area is entirely within the City's Sphere of Influence, which means that it is land that is ultimately planned for annexation into Rohnert Park. Other government agencies with jurisdiction in the Plan Area include:

- Regional Water Quality Control Board
- State of California Department of Fish and Game
- Army Corps of Engineers
- United States Fish and Wildlife Service

It is intended that the City of Rohnert Park will seek annexation of the Plan Area into the City following adoption of the Specific Plan.

Chapter Three

NORTHWEST SPECIFIC PLAN AREA VISION

This chapter describes the overall vision for the Plan Area. It presents the Land Use Concept and some of the primary recommendations for the Plan Area. The Land Use Concept diagram in this chapter is illustrative only and is not intended as zoning or as official land use designations. Land use designations and zoning standards are provided in Chapters Four and Five, respectively.

Through an extensive public engagement process, the community and landowners made it clear through the process that the Plan Area should be developed with an attractive mix of commercial uses, new higher density residential uses (especially north of the busy arterial of Golf Course Drive West), and a limited amount of light industrial uses. As shown on Figure 3-1, the Illustrative Land Use Plan envisions a mix of all these uses to be served primarily by Dowdell Avenue and Golf Course Drive West. This section provides an overview of the overall land use vision for the Northwest Specific Plan Area.

HOW TO USE THIS SECTION

This chapter is intended to “tell the story” of the future of the Northwest Specific Plan Area. The reader should use this section as a means to understand the major community goals for the area and the planning and design concepts proposed by this Specific Plan to achieve those goals.

CONCEPTUAL LAND USES

The Illustrative Land Use Plan (Figure 3-1) shows a broad-level representation of the land uses envisioned for the Plan Area by dividing the Plan Area into conceptual land use districts. The Illustrative Development Plan (Figure 3-2) depicts a conceptual physical buildout based on this vision. Table 3-1 shows a conceptual development program for overall square footage of new development categorized by use. The development program shown serves as a basis for program level technical analysis required as part of the State-required environmental review process. The program is conceptual in nature and may not be built out exactly as depicted over the next twenty years.

Figure 3-1: ILLUSTRATIVE LAND USE PLAN

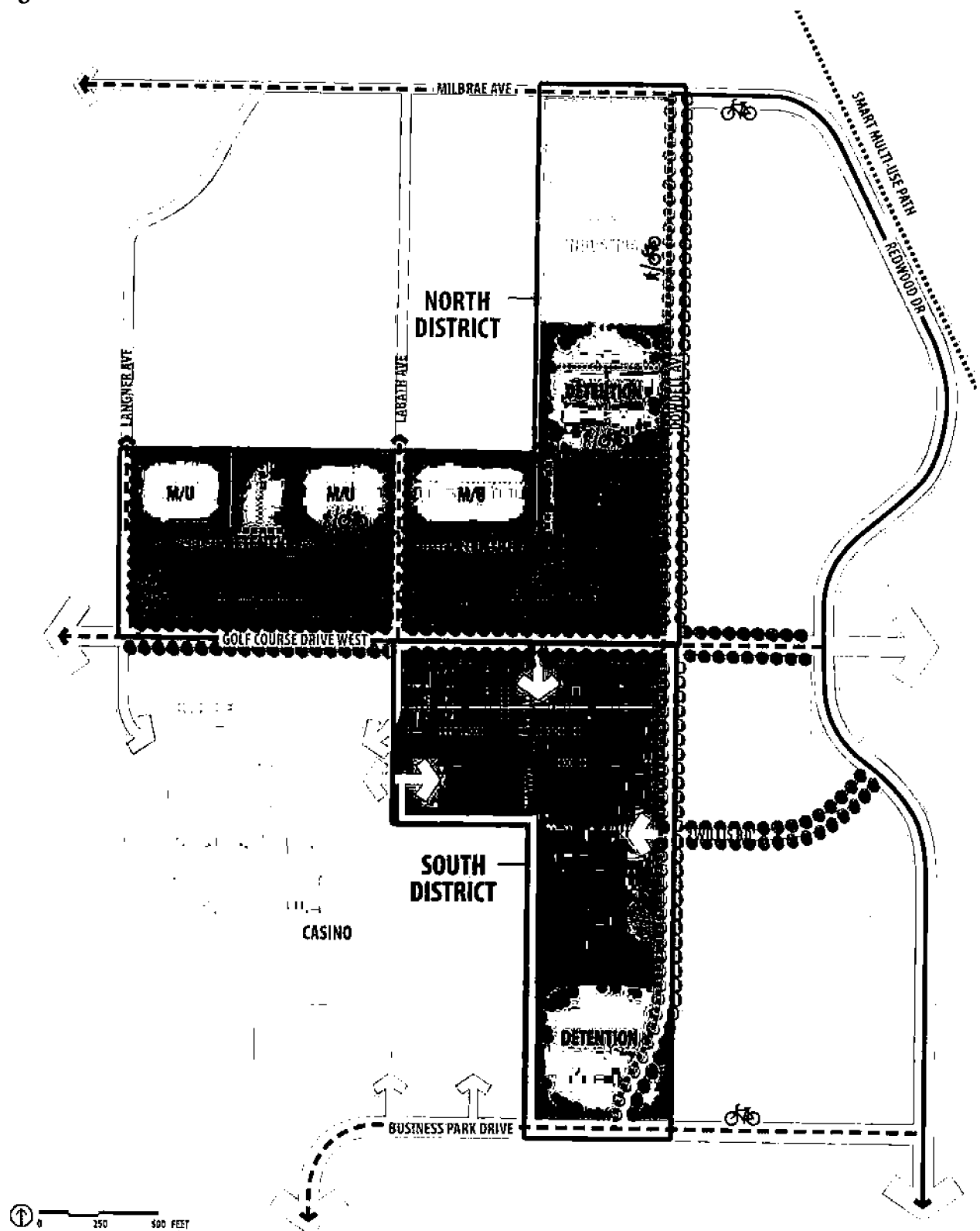
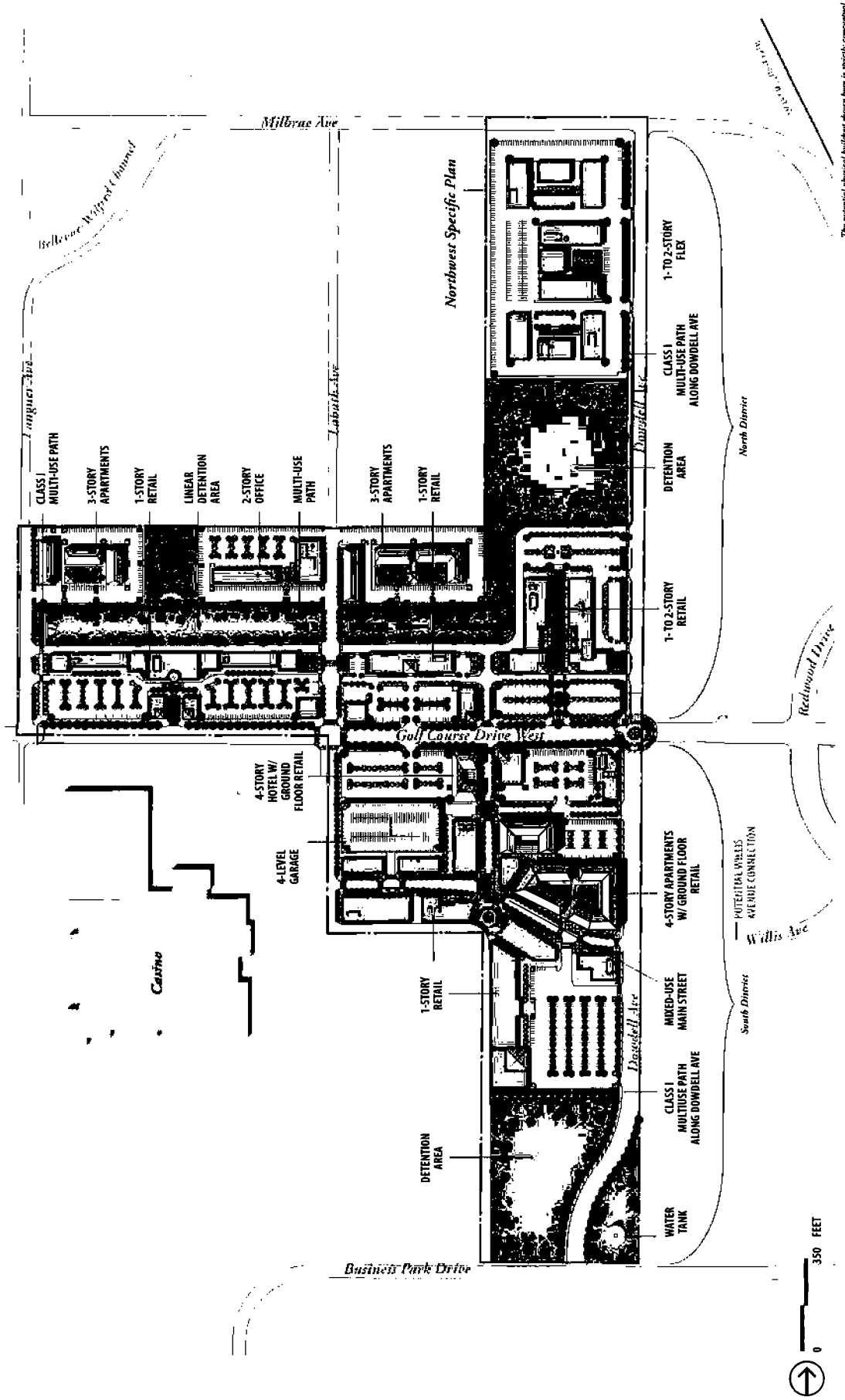


Figure 3-2: ILLUSTRATIVE DEVELOPMENT PLAN



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MIXED USE SOUTH

The Plan Area is envisioned as having a significant amount of mixed-use development, which could include a variety of retail, office, hotel, and residential uses. Mixed-use is envisioned for almost all of the land in the south district and also for some of the properties in the north district.

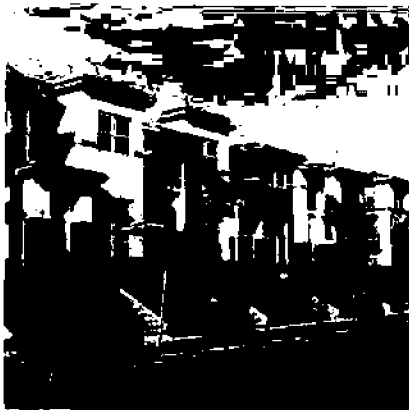
As shown in the Illustrative Development Plan (Figure 3-2), new mixed-use development in the south district is envisioned to occur around an interior “main street.” It is envisioned that mixed-use areas in the south district would include a mix of pure retail and entertainment uses in single-story buildings, as well as ground floor retail and entertainment uses within multi-story buildings that integrate residential, hotel, or office uses on upper floors. The character of this area is envisioned to have a “main street” feel with active and transparent ground floor frontages, wide sidewalks, public plazas, and tree-lined streets with parallel on-street parking.





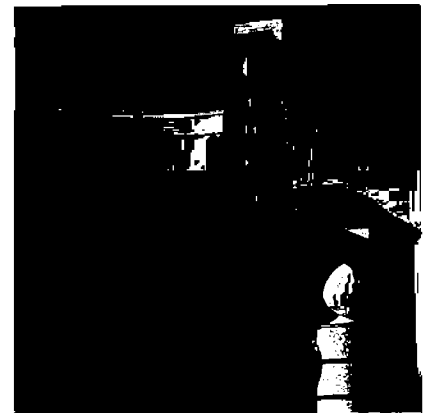
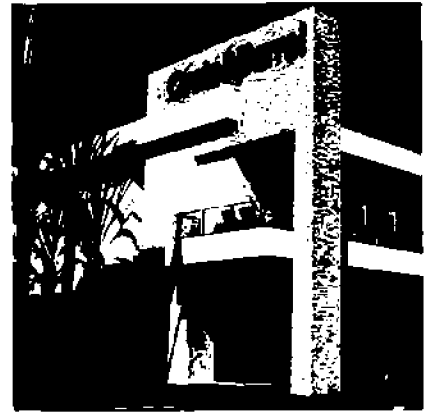
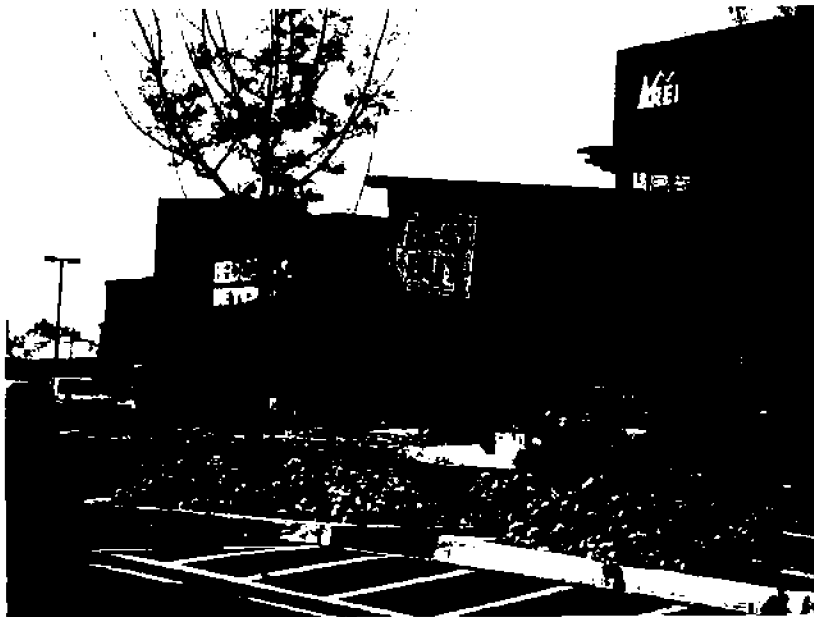
MIXED USE NORTH

In the north district, the mix of uses is envisioned to include higher density residential uses, small offices (such as a medical), and a limited amount of retail uses. It is anticipated that mixed-use areas in this area are more likely to accommodate a horizontal mix of uses where different uses may occur side-by-side. Upscale Recreational Vehicle (RV) camping and other uses may be appropriate as an interim use in these areas.



RETAIL

A commercial area is envisioned for parcels that front the north side of Golf Course Drive West. This land use is intended to ensure that new development takes advantage of its visible location along Golf Course Drive West, but commercial uses at this location also serves as a buffer between the planned casino and potential residential uses that may occur in the mixed-use areas in the north district. The regional-serving stores, or other uses envisioned, would take advantage of the visibility from Golf Course Drive West. The retail format envisioned for the north district generally requires visible and ample parking dedicated to each building. Commercial areas could also potentially accommodate regional-serving entertainment uses, such as live theater, music venues, IMAX theater, or similar uses.





FLEX

The northernmost section of the north district, which fronts Millbrae and Dowdell Avenue, is envisioned as a self-contained flex district that could include a mix of office, commercial service, industrial, or light industrial buildings. Flex uses are intended to serve and supplement the proposed adjacent retail and entertainment uses. Examples may include businesses such as caterers or commercial laundry services, but more traditional industrial uses and offices are envisioned to be appropriate in this area as well.



CONCEPTUAL DEVELOPMENT PROGRAM

Table 3-1 below provides a conceptual estimate of the potential net new dwelling units and commercial square footage that is expected to occur in the Plan Area through buildout. Buildout is defined as a future year when all parcels that are called for new development in the Specific Plan have been fully developed and none are left vacant or undeveloped. The development program is a projection and may occur differently as actual projects come forward.

Table 3-1: CONCEPTUAL DEVELOPMENT PROGRAM

Land Use Type	Dwelling Units (du) or Square Feet (sf)
Residential	398 du
Office	58,400 sf
Office/Light Industrial	218,200 sf
Commercial	458,700 sf
Hotel	100 rooms
Parks/Recreation*	4.8 Acres (209,088 sf)

*To be provided as part of open space, trail system, and adjacent to detention areas throughout the Plan Area

ADDITIONAL LAND USE COMPONENTS

RAINWATER DETENTION

Significant stormwater detention areas will be required to accommodate new impervious surfaces created with the development of the Plan Area. Detention is envisioned as being divided into three sections. All of the parcels in the south district are envisioned to drain into one detention area in the southernmost portion of the Plan Area. In the north district, commercial uses and mixed-uses on the western portion are envisioned to drain onto an approximately 100' wide linear detention area between Langner Avenue and halfway-between Labath and Dowdell Avenue. Commercial uses and flex uses in the east portion of the north district are envisioned to drain onto a detention area midway between Golf Course Drive West and Millbrae Avenue. The conceptual detention areas are envisioned to occur on portions of multiple parcels and near the three southern, western, and northern ends of the Plan Area – Business Park Drive, Langner Avenue, and Millbrae Avenue – as much as possible to take advantage of current water flow. In the south district, detention is shown at the furthest southern part of the Plan Area. However, the areas shown for detention are conceptual in nature and are not yet determined. Exact locations for detention basins will be determined as properties develop and in coordination with landowners. More information regarding conceptual financing strategies related to detention areas can be found in Chapter 9, Implementation and Financing.



PARKS AND RECREATION

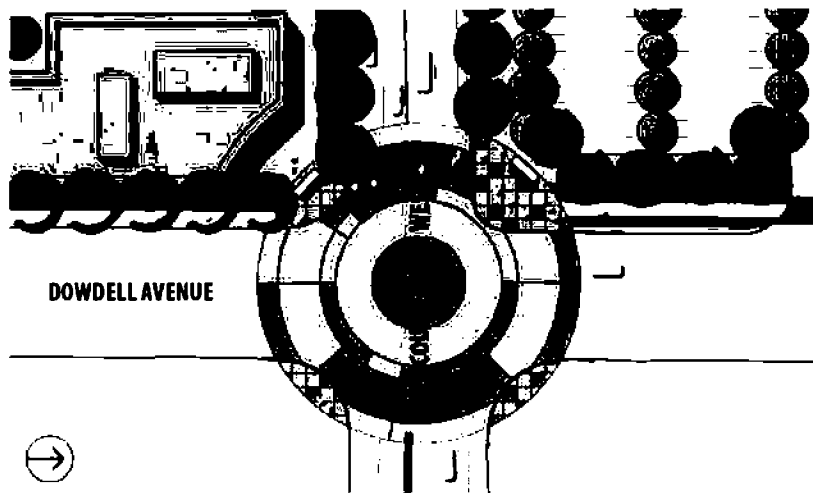


An east-west trail is envisioned to connect Langner Avenue to Dowdell Avenue, including along the linear detention areas shown on Figure 3-2. This east-west trail will connect to a proposed Class I multi-use path on the east side of Langner Avenue connecting users south to Golf Course Drive West. The eastern terminus of the east-west trail will intersect with a proposed Class I multi-use path on the west side of Dowdell Avenue that connects Millbrae Avenue to Business Park Drive. In dry seasons, the detention areas are envisioned to function as open space, or even recreation areas, subject to City approval. The estimated buildout of the Specific Plan would require 4.8 acres of park land per the General Plan. The Illustrative in Figure 3-2 illustrates how a portion of this could be achieved through a conceptual 1 to 2 acre park associated with potential residential uses in the north district; the exact location and design of the 4.8 total acres of park land will be determined as projects come forward, in coordination with landowners.

CIRCULATION, STREETScape AND TRAIL CHARACTER

DOWDELL AVENUE/GOLF COURSE DRIVE WEST GATEWAY

This intersection provides direct access to the center of the Plan Area from Highway 101. The intersection should be treated as a major gateway to the Northwest Specific Plan Area. To the extent feasible, special design features should be used to signify this entry. This could be executed in a variety of ways, including with landscaping, special signage, special building features, special pavers and pavement colors, or other similar features, as shown in the drawing below. New development projects located in this area should incorporate design features that accentuate this intersection. Depending on the timing of future projects, coordination with the Wilfred/Dowdell Village Specific Plan development along Golf Course Drive West should be considered to achieve one major gateway feature signifying entry to northwest Rohnert Park. Depending on the wishes of project proponents, gateway features could be appropriate at both locations.



OTHER POTENTIAL GATEWAYS

Other potential gateway features may also be appropriate to signify entry into the Northwest Specific Plan Area, however these may be more minor than is recommended for Golf Course Drive West/Dowdell Avenue. It is recommended that gateway features at other entry points be limited to special landscaping or monument signage located at the corners within the setbacks in private development. Gateway locations to be considered include Millbrae Avenue/Dowdell Avenue, Business Park Drive/Dowdell Avenue, and Langner Avenue/Golf Course Drive West.

GOLF COURSE DRIVE WEST CORRIDOR

The Golf Course Drive West corridor is the primary roadway that will bring vehicular, pedestrian, and bicycle traffic in and out of the Plan Area. New development along Golf Course Drive West should include features that are oriented toward the road. New commercial development on the north side of Golf Course Drive West should face the road. Where buildings are set back from the street, generous landscaping should be provided within setbacks to provide visual interest along the road and highlight its importance. In the south district, located south of Golf Course Drive West, the Specific Plan envisions that new mixed-use development will be oriented toward a new interior roadway. This may result in some surface parking and smaller retail uses being located along Golf Course Drive West in the south district. Where this occurs, generous landscaping and street trees should be provided along the roadway to minimize views to surface parking from Golf Course Drive West.



WILLIS AVENUE ENTRY

The Wilfred/Dowdell Village Specific Plan, located to the east of Dowdell Avenue, south of Golf Course Drive West, and west of Redwood Drive, proposes a new internal circulation network that would extend Willis Avenue from its current location to Dowdell Avenue. The Northwest Specific Plan recognizes this proposed intersection and proposes a special entry to the south district be located to align there. The exact location is likely to vary given development constraints at the project level, but it is preferable that a new special intersection be created at this location to the extent feasible. This will help tie the two Specific Plan Areas together and create a network that ties into Redwood Drive to the east.

LABATH AVENUE AND LANGNER AVENUE DEVELOPMENT

Two new mixed-use areas are proposed adjacent to Labath Avenue and a third is proposed adjacent to Langner Avenue. These developments are envisioned to be separated from the new commercial areas along Golf Course Drive West by a new linear detention basin, as shown in the drawing on this page. It is proposed that development in these mixed-use areas be designed to orient toward either Labath Avenue or Langner Avenue. New development should provide entries that face these roadways, as well as windows, significant architectural features, and building articulation. Given the shape of these mixed-use areas and their minimal frontage along these roads, it is likely that there will be additional buildings located further away from these roads as well. New landscaping should be provided within building setbacks to provide a buffer from residential development and a visually-pleasing streetscape.

TRAIL SYSTEM

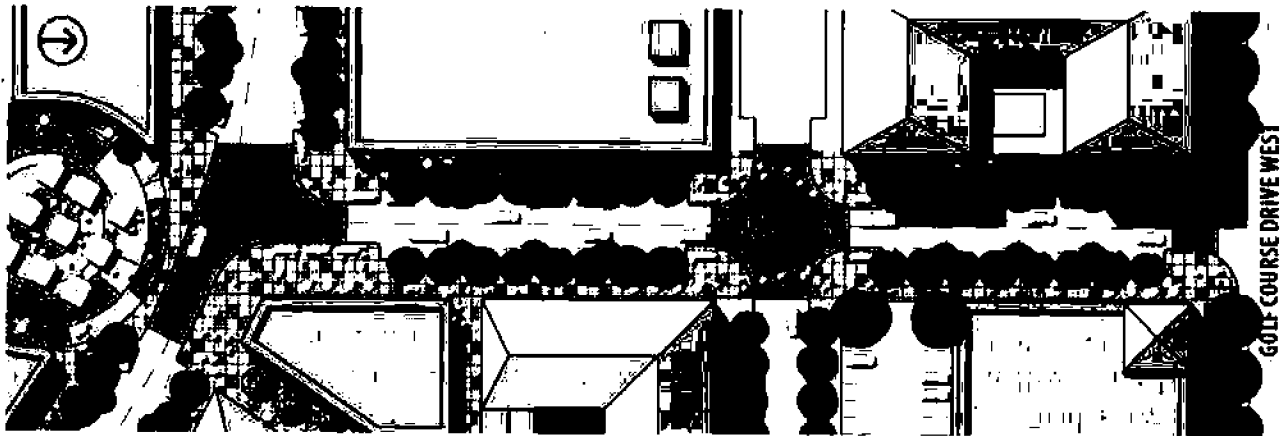
Pedestrian travel is proposed along roadways throughout the majority of the Plan Area. Every improved road shall have sidewalks on both sides and provision for bicycle travel.

In addition, a new pedestrian and bicycle trail is proposed to be provided along with new development that would connect Langner Avenue to Dowdell Avenue through the Plan Area, as shown in the drawing on this page. It is proposed that a new trail be provided along the proposed detention areas and in concert with new development. This amenity for employees and residents in the area would provide for improved non-motorized circulation. The new trail should and incorporate landscaping at key locations, mid-block crossings at intersections with roadways, and appropriate lighting, as necessary. The trail system is shown in a conceptual location within the context of the entire Plan Area on Figure 3-2. The exact location will need to be determined.



MIXED-USE MAIN STREET

A new, pedestrian-oriented “main street” is envisioned to connect Golf Course Drive West to the Willis Avenue entry as a means to provide pedestrian and vehicular circulation and access through the south district. This is shown conceptually in Figure 3-2: Illustrative Plan; however, this new street could take different forms and configurations. As shown in the drawing below, the “main street” is the spine of the south district, with retail, hotel, and residential uses facing onto it. The street is envisioned as a narrow, cozy environment where cars are encouraged to slow down. Wide sidewalks, outdoor seating, and parallel parking are proposed to emphasize pedestrian activity.



BICYCLE CIRCULATION SYSTEM

The Northwest Specific Plan Area is envisioned to accommodate all mode of travel, including bicyclists. As mentioned above, the Specific Plan recommends that all new roads accommodate bicycle travel, either with bicycle lanes or with off-street Class I bicycle paths. The existing and recommended bicycle facilities for the Northwest Specific Plan Area and surroundings are discussed in detail and shown in Chapter 6, Figure 6-3.

The Plan Area bicycle circulation system is envisioned to be accommodated primarily on Golf Course Drive West, Dowdell Avenue, and Business Park Drive. Bicycle lanes currently exist on Golf Course Drive West. The Plan designates future bicycle lanes on Business Park Drive and an off-street path along Dowdell Avenue. These recommendations are consistent with the Countywide Bicycle and Pedestrian Master Plan in most cases. In some cases, this Specific Plan proposes bicycle facilities more significant than are called for in the Bicycle and Pedestrian Master Plan. As shown on Figure 6-3, a continuous Class I bike path is proposed along Dowdell Avenue and east-west through the north district that will fully connect the area. Additionally, there are several opportunities that should be explored to create connections to the future SMART Multi-Use Path along the SMART rail line.



Chapter Four

LAND USE

This chapter describes the land use designations applicable within the Northwest Specific Plan Area. Parcels in the Plan Area are designated with existing Rohnert Park General Plan Land Use Designations. Figure 4-1 shows land use designations and associated acreages for the Plan Area. The map and acreages as shown reflects the most recent parcel information at the time of this Plan preparation, and assumes that the proposed Dowdell Avenue realignment as it approaches Business Park Drive as completed.

HOW TO USE THIS SECTION

This chapter is intended to provide the reader with the Land Use Designations for lands in the Plan Area. The reader should review the land use designations in this section to gain an overall understanding of the broad uses and building densities and intensities that are allowed in various subareas within the Northwest Specific Plan Area.

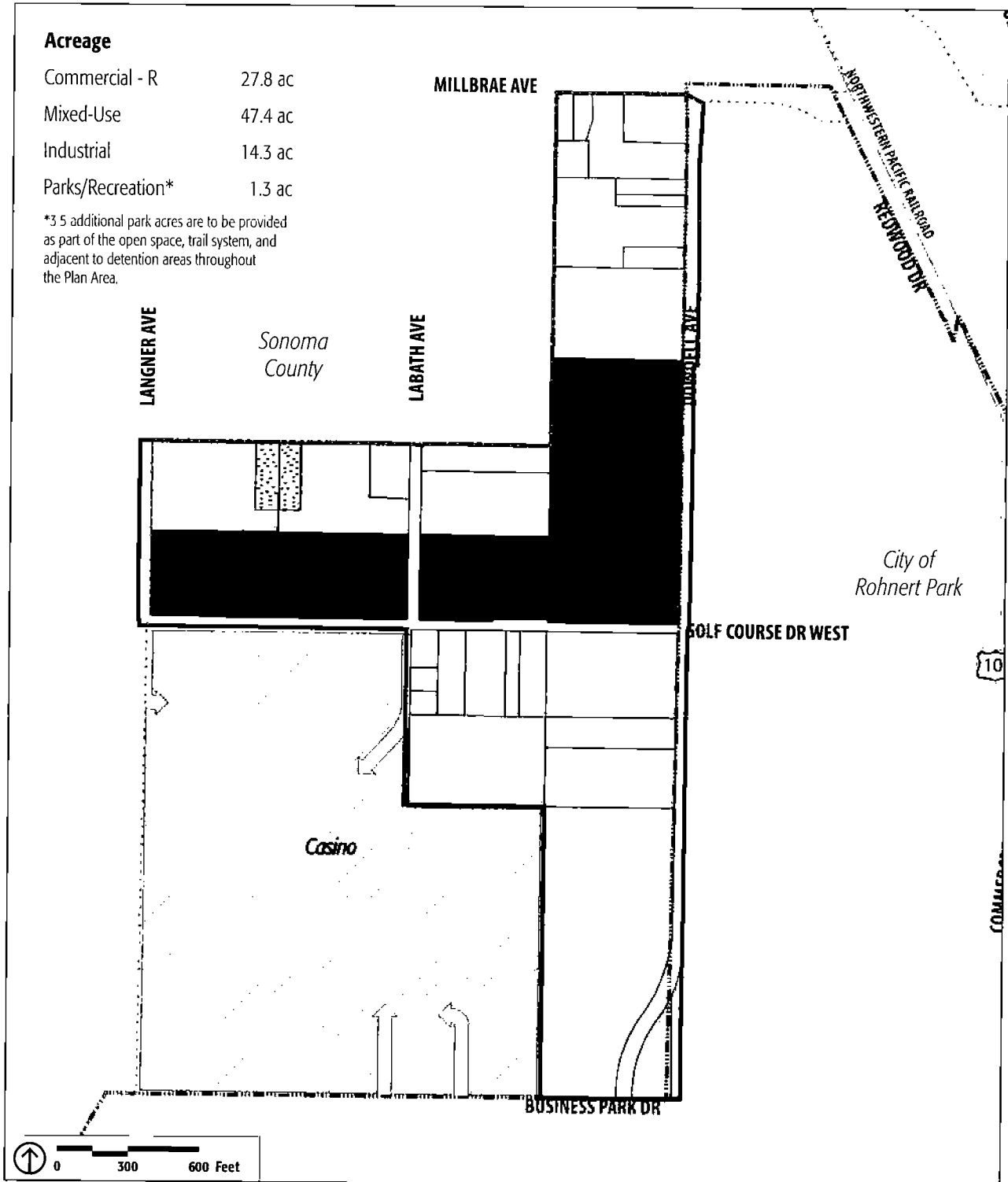
LAND USE DESIGNATIONS

The classifications in this section represent adopted City policy. They are meant to be broad enough to give the City flexibility in implementing this City policy in the Northwest Specific Plan Area, but clear enough to provide sufficient direction to achieve the overall vision for the Plan Area. The next chapter and the City's zoning ordinance contain more detailed provisions and standards to implement these classifications.

MIXED-USE

This designation accommodates a variety of compatible businesses, stores, institutions, service organizations, hotels, and residences in a pedestrian-oriented setting. Allowable uses include multi-family residences (up to 24 dwelling units per acre), retail shops, financial, business and personal services, and restaurants. This designation is intended to accommodate these uses in a pedestrian-oriented environment that provides access to residents' basic everyday needs and is designed to promote walkability. Uses in this area may be accommodated in the same building or horizontally mixed. Maximum floor-area ratios

Figure 4-1: LAND USE DESIGNATIONS



- | | | |
|---------------------------|------------------------------|------------------|
| Northwest Specific Plan | Land Use Designations | Industrial |
| City Boundary | Commercial - R | Parks/Recreation |
| Sphere of Influence (SOI) | Mixed-Use | |

for commercial uses are allowed up to 1.5 for commercial and mixed-use projects that do not include residential. Mixed-use projects containing residential and commercial uses are allowed to a total FAR of 2.0.

COMMERCIAL-R

This designation is intended to provide sites for retail areas containing a wide variety of businesses, including: retail stores, eating and drinking establishments, commercial recreation, financial, business and personal services, hotels and motels, and educational and social services. Maximum permitted FAR is 1.5 for hotels and 0.4 for all other uses. Shopping centers typically include department stores or big-box stores, which attract consumers from outside the city.

INDUSTRIAL

This designation accommodates campus-like environments for corporate headquarters, research and development facilities, offices, light manufacturing and assembly, industrial processing, general service, warehousing, storage and distribution, and service commercial uses. Retail is permitted as an ancillary use only. Maximum FAR is 0.5, but discretionary increases may be permitted up to a total FAR of 1.0.

PARKS/RECREATION

This designation provides for parks for active and passive recreation, recreation complexes, community fields, public golf courses, stadiums, arboretums, and greenways. Ancillary facilities such as concession stands, clubhouses, and equipment rental are also allowed.

LAND USE POLICIES

The following policies are intended to be considered when reviewing proposed projects within the Specific Plan Area. Though intended to guide the implementation of the vision of the Plan, in some cases the policies *must* be followed for new development to achieve the overall vision and to respect existing physical surroundings.

LAND USE COMPATIBILITY AND COMMUNITY CHARACTER

- Policy LU-1.1 New residential development shall not front Dowdell Avenue.
- Policy LU-1.2 New residential development is encouraged to line the western boundaries of the North District to take advantage of views to the west.
- Policy LU-1.3 Developers in the Mixed-Use areas in the North District are encouraged to consider smaller retail components given the distance from Golf Course Drive West. However, if market conditions support such a use, retail uses must be buffered from pre-existing residential uses, using screening methods described in the City of Rohnert Park Design Guidelines.
- Policy LU-1.4 New commercial, mixed-use, and multi-family development must follow the current City of Rohnert Park Design Guidelines for Commercial, Mixed-use, and Multi-family Buildings approved by the Rohnert Park City Council.
- Policy LU-1.5 New commercial development adjacent to Golf Course Drive West must provide at least one accentuated entry driveway off of Golf Course Drive West.
- Policy LU-1.6 New development adjacent to Golf Course Drive West is encouraged to locate parking areas away from the street frontage. If parking areas along the street are necessary, they shall be screened from Golf Course Drive West by the consistent planting of trees and appropriate landscaping measures. Commercial pads of at least 7,500 square feet shall be placed along the street for every 500 linear feet of parking frontage.

OPEN SPACE

- Policy LU-1.7 New development shall provide new park uses per citywide requirements, preferably including a park in the northern area associated with residential uses.

PARKING

- Policy LU-1.8 New mixed-use development should create a parking assessment district to accommodate increased parking demand and allow flexible arrangement of parking for compact development.

HOUSING

- Policy LU-1.9 Encourage multi-family and mixed-use housing in the Plan Area as a means to bring increased vitality and provide additional affordable housing options.

STORMWATER RUNOFF

- Policy LU-1.10 Encourage development to minimize impervious surfaces and other mechanisms to reduce stormwater runoff.

Chapter Five

PERMITTED LAND USES & DEVELOPMENT STANDARDS

This chapter discusses the permitted, conditional, and prohibited land uses, and the development standards for each land use designation provided in the previous chapter, including Commercial, Mixed-Use, and Industrial. Refer to the City's Zoning Code for permitted uses and regulations regarding Parks/Recreation uses. The development standards establishes the requirements for new development and takes precedence over the City's zoning ordinance in the case of any conflict. These standards are intended to implement the vision outlined in Chapter Three. For any issues not specifically addressed by the development standards in this Specific Plan, please refer to the City of Rohnert Park Zoning Code. In the case of a conflict with the Zoning Code, this Specific Plan's development standards apply. The most compatible zoning designation should be used for such issues as determined by the Planning Director.

HOW TO USE THIS SECTION

This chapter is intended to provide the reader with an understanding of the specific uses allowed in the Plan Area and the applicable standards for new development in the Plan Area.

- Table 5-1: Permitted Uses. The reader should use this section to determine what specific uses would be allowed within a new potential new project in the area. This table establishes permitted, conditional, and prohibited uses in the various districts in the Plan Area. Uses not shown are prohibited unless determined by the Planning Director to be consistent with the Specific Plan.
- Table 5-2: Development Standards. The reader should use this section to understand how a building must be designed respective to building height, building placement on a site, landscaping, setbacks, and other areas.

COMMERCIAL-R

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P - Permitted

C - Conditionally-permitted by Planning Commission

A - Administrative Permit

Z - Certificate of Zoning Compliance

T - Temporary Conditional Permit

I - Uses Allowed as Incidental to a Primary Use

If the listed land use is followed by a letter or a section reference in parenthesis, that number or reference directs the reader to the corresponding land use footnote or special provision in Municipal Code Section 17.07.020.

Table 5-1: PERMITTED USES

Land Use Category	C-R
Amusement Center (e.g. miniature golf, golf driving range, bowling alley, cyber cafe) (B)	
▪ Small (e.g., indoors; commercial shopping center)	A
▪ Large (e.g., indoors or outdoors; stand alone facility)	C
Animal Hospital/Veterinary Clinic	C
Antique Store	P
Arcade Games/Cybercafes (B)	P/A
Automobile Service Station (C)	C
Bakery (Retail Sales)	P
Bank/Savings & Loan/Credit Union (see Drive-Through Windows) (I)	P
Bar/Nightclub (R)	C
Barber/Beauty Shop/Tanning Salon	P
Bath House/Spa	C
Billiards Parlor (R)	P
Broadcasting Studio	C
Clubs & Lodges	C
Commercial Filming Studio	C
Communication Facility (F)	C/A
Convention Center	C
Cultural Institution (e.g. museums)	C
Day Care Center (Non-Residential)	C
Drive-Through Window (any use) (I)	C
Drive-Through Window (pharmacy) (I)	C
Dry Cleaning Outlet	P
Firearm Dealers and Firearm Ammunition Dealers (J)	C
Florist	P
Food Store	
▪ Convenience Store	C
▪ Supermarket	C
Furniture Store	
▪ Small/Custom Order	C
▪ Large	C
Health Club	A

Land Use Category	C-R
Homeless Shelter (M)	
▪ Small (6 or less persons)	P
▪ Large (7 or more persons)	P
Hotel/Motel (No in-room food preparation unless applied for and approved as part of project approval or separately.)	P
Interior Decorator	P
Kennel (Commercial) (O)	C
Laboratory	
▪ In conjunction with a medical, dental or optical use	P(I)
Laundromat	
Liquor Store (Off-Sale) (R)	C
Live Entertainment	C
Massage Therapy (see Chapter 9.80 of Zoning Code)	P
Microbrewery with restaurant	P
Office	
▪ Professional and Administrative	C
▪ Medical and Dental	C
Parking Lot (Commercial)	C
Pharmacy (see Drive-Through Window) (I) (Does not include a Medical Marijuana Dispensary, which is a prohibited use within the City.)	P
Photography Studio	P
Printing & Blueprinting	
▪ Small Copy Center	P
▪ Print Shop	P
Private/Public Utility Facility (F)	
▪ Minor	Z/C
▪ Major	C
Public Assembly	A
Public Facility-Non-city owned or proposed (see also Public Utility)	P
Public Facility-City owned or proposed (subject to Planning Commission review on referral from City Council)	P

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Table 5-1: PERMITTED USES

Land Use Category	C-R
Recovery Facility	
▪ Small (6 or less persons)	A
▪ Large (7 or more persons)	C
Recycling Facility (V)	
▪ Reverse Vending Machines	P
▪ Small Collection Facility	A
Religious Assembly	C
Residential Care Facility	
▪ Congregate Care/Assisted Living	C
▪ Convalescent Hospital	C
▪ Senior Housing (Independent Living)	C
Restaurant	
▪ General	P
▪ Fast Food (see also Drive-Through Window- I)	C
▪ Outdoor & Sidewalk Cafe (S)	A
▪ Take Out/Delivery	P
▪ With Bar & Live Entertainment (R)	C
Retail, General and Specialty	P
▪ Department or Big Box Retail	P
Retail Warehouse Store (e.g., big box)	C
Sign Shop	
▪ Small (e.g., typically located in a small office/retail space)	C
▪ Large	C
Single Room Occupancy Living Unit Facility (Z)	A
Single Room Occupancy Residential Hotel (Z)	A
Studio (e.g. Dance, Martial Arts)	P
Tailor	P
Tattoo/Piercing Studio	P
Telecom Center	
Temporary Use/Event (EE, see also DD)	
▪ Arts & Crafts Show	T
▪ Circus/Carnival	T
▪ Flea Market/Swap Meet	A

Land Use Category	C-R
▪ Live Entertainment	A
▪ Outdoor Exhibit	T
▪ Recreation Event	A
▪ Religious Assembly	A
▪ Retail Sales	T
▪ Seasonal Lot/Activity (e.g. Christmas trees, pumpkins)	T
▪ Trade Fair	T
Theater	C
Thrift Shop	P
Transit Facility	C
Upholstery Shop	A

COMMERCIAL-R

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MIXED-USE

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Table 5-1: PERMITTED USES

Land Use Category	M-U
Amusement Center (e.g. video games, other indoor amusements)	C
Antique Store	C
Bakery (Retail Sales)	C
Bank/Savings & Loan/Credit Union (drive-through windows not permitted)	C
Bar/Nightclub (R)	C
Barber/Beauty Shop	C
Bath House/Spa	C
Bed & Breakfast Inns (D)	C
Billiards Parlor (R)	P
Communication Facility (F)	C/A
Cultural Institutions (e.g. museums)	A
Day Care Center (Non-Residential)	C
Dry Cleaning Outlet	P
Florist	P
Food Store	
▪ Under 15,000 square feet	P
▪ Between 15,000 sq. ft. and 40,000 sq. ft.	C
Health Club	P
Hotel (100 rooms or less) (No food preparation unless applied for and approved as part of project approval or separately.)	C*
Interior Decorator	P
Laboratory	
▪ In conjunction with a medical, dental or optical use	P(I)
Laundromat	P
Liquor Store (Off-Sale) (R)	C*
Live Entertainment (R)	C*
Massage Therapy (see Chapter 9.80 of Zoning Code)	P
Medical Clinic	A
Microbrewery (with restaurant)	C*
Office	
▪ Professional and Administrative	P

*Not Allowed in the north district

Land Use Category	M-U
▪ Medical and Dental	P
Pharmacy (drive-through windows not permitted) (Does not include a Medical Marijuana Dispensary, which is a prohibited use within the City.)	P*
Photography Studio	P
Printing	
▪ Small Copy Center	P
Public Assembly	C
Public Facility (e.g. police and fire stations, community centers, government offices)	C
Religious Assembly	A
▪ Homeless shelter (6 or less persons)	P
Residential Facility	
▪ Congregate Care/Assisted Living	A
▪ Convalescent Hospital	A
▪ Single Room Occupancy Living Unit Facility (Z)	P
▪ Single Room Occupancy Residential Hotel (Z)	P
▪ Senior Housing (Independent Living)	P
Residential Use	
▪ Live/Work (P)	P
▪ Multi-Family	P
▪ Townhouse	P
Restaurant	
▪ General	P
▪ Outdoor & Sidewalk Café (S)	A
▪ Take Out/Delivery (drive-through windows not permitted)	P
▪ With Bar & Live Entertainment (R)	C
Retail, General and Specialty	P
School	
▪ Elementary or Secondary	C**
▪ High School	C**
▪ Trade School	C**
▪ College	C**
Studio (e.g. Dance, Martial Arts)	A

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*Not Allowed in the north district

**Only in the north district. Not Allowed in the south district.

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Table 5-1: PERMITTED USES

Land Use Category	M-U
Tailor	P
Tattoo/Piercing Studio	C
Temporary Use/Event (EE; see also DD)	
▪ Arts & Crafts Show	T
▪ Circus/Carnival	T
▪ Flea Market/Swap Meet	T
▪ Live Entertainment	T
▪ Outdoor Exhibit	T
▪ Recreational Event	T
▪ Religious Assembly	C
▪ Retail Sales	T
▪ Seasonal Lot/Activity (e.g. Christmas trees, pumpkins)	T
▪ Trade Fair	T
Theater (under 500 seats)	C*
Transit Facility (e.g. bus or train station)	C
Visitor Center	P

*Not Allowed in the north district

Land Use Category	I-L
Agricultural Processing (includes viticulture)	C
Agricultural Services	A
Aircraft Related Industry	A
Animal Hospital/Veterinary Clinic	A
Ambulance Service	A
Appliance Repair Service	P
Auto Parts Sales & Installation	P
Automobile Service Station (C)	C
Beverage Bottling Plant	P
Boat, RV, and Outdoor Storage Facility (E)	C
Boat Building	P
Brewery/Distillery/Winery (R)	A
Broadcasting Studio	A
Car Wash	P
Clubs & Lodges	C
Commercial Filming Studio	P
Communication Facility (F)	C/A
Contractors' Storage Yard	C
Convention Center	C
Cooperage	P
Cultural Institution (e.g. museums)	C
Dairy Products Processing	P
Day Care Center (Non-Residential)	C(I)
Dry Cleaning Plant	A
Exterminator	P
Food Processor	C
Fuel Storage	C
Funeral Parlor/Mortuary	C
Health Club	P(I)
Household Hazardous Waste Facility	C
Household Services/Contractors (e.g. plumbing, painting, electrical, interior decorating)	P
Kennel (Commercial) (O)	C
Laundries/Linen Supply Service	P

INDUSTRIAL

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Table 5-1: PERMITTED USES

Land Use Category	I-L
Light Manufacturing and/or Assembly (Laboratory requirements to Biosafety Levels 1 and 2)	P
Lumber Yard	P
Massage Therapy (see Chapter 9.80 of Zoning Code)	P(I)
Medical Laboratory	A
Microbrewery	
▪ with restaurant	C
▪ without restaurant	P
Nursery (Horticulture)	P
Office	A
Parking Lot (Commercial)	C
Parcel Delivery Service	P
Personal Services	
▪ As a Principal Use	A
▪ As an Incidental Use	I
Photographic Plant	P
Printing & Blueprinting	P
Private/Public Utility Facility (F)	
▪ Minor	Z/C
▪ Major	C
Public Assembly	C
Public Facility—Non-City owned or proposed (see also Public Utility)	C
Public Facility-City owned or proposed (subject to Planning Commission review on referral from City Council)	P
Publishing	P
Recycling Facility (V)	
▪ Reverse Vending Machines	P
▪ Small Collection Facility	A
▪ Large Collection Facility	C
▪ Light Processing Facility	C
Religious Assembly	C
Research and Development (Laboratory requirements to Biosafety Levels 1 and 2 only)	P
Restaurant (I)	

Land Use Category	I-L
▪ As an Incidental Use	A
Retail Use	
▪ As an Incidental Use	A
Retail Warehouse	C
School	
▪ Trade School	A
▪ High School	C
Security Guard Residence	I
Self-Storage Facility (Y)	C
Sign Shop	P
Stone Works	P
Studio (e.g. Dance, Martial Arts)	A
Taxidermist	A
Telecom Center	A
Temporary Use/Event (EE, see also DD)	
▪ Arts & Crafts Show	T
▪ Circus/Carnival	T
▪ Flea Market/Swap Meet	A
▪ Live Entertainment	A
▪ Outdoor Exhibit	A
▪ Recreational Event	A
▪ Religious Assembly	A
▪ Retail Sales	A
▪ Seasonal Lot/Activity (e.g. Christmas trees, pumpkins)	T
▪ Trade Fair	T
Towing Service/Impound Yard	A
Trucking Terminal (including moving & storage)	A
Upholstery Shop	P
Vehicle Storage Yard	A
Vehicular Dealerships/Rentals (incl. boats, farm & construction equip.) (FF)	A
Vehicle Repair/Body Shops (GG)	P
Warehousing/Wholesaling	P

INDUSTRIAL

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I - Uses Allowed as Incidental to a Primary Use

If the listed land use is followed by a letter or a section reference in parenthesis, that number or reference directs the reader to the corresponding land use footnote or special provision in Municipal Code Section 17.07.020.

Development standards in Table 5-2 are specific to the districts in the Northwest Specific Plan Area. The districts are shown in Figure 4-1 of Chapter 4, and include Commercial-R, Mixed-Use, and Industrial. Refer to the City's Zoning Code for regulations regarding Parks/Recreation uses. For any issues not specifically addressed by the development standards in this Specific Plan, please refer to the Zoning Code. In the case of a conflict with the Zoning Code, this Specific Plan's development standards apply. The most compatible zoning designation should be used for such issues as determined by the Planning Director.

Table 5-2: DEVELOPMENT STANDARDS

Development Requirement	Commercial-R	Mixed-Use	Industrial
Maximum Residential Density (du/acre)	N/A	24	N/A
Maximum Building Height - Primary Structure (vertical feet) ¹	35 or 2 Stories	46 or 4 Stories	35 or 2 Stories
Maximum Building Height - Accessory Structure (vertical feet) ²	16	16	16
Minimum Setback - Dowdell Avenue (feet)	15	N/A	20
Minimum Setback - Golf Course Drive West (feet)	10	10	N/A
Minimum Setback - Labath Avenue (feet)	5	5	N/A
Minimum Setback - Langner Avenue (feet)	15	15	N/A
Minimum Setback - Milbrae Avenue (feet)	N/A	N/A	15
Maximum Setback - Future Interior Mixed-Use Street (feet) ³	0-10	0-10	N/A
Minimum Side Setback - Interior (feet)	10	0	15
Minimum Side Setback - Exterior (feet)	5	0	20
Minimum Rear Setback - Primary Structure (feet)	10	10	15

¹ Towers, spires, cupolas, chimneys, elevator penthouses, water tanks, monuments, and similar structures and necessary mechanical appurtenances covering not more than twenty percent of the top floor roof area may exceed by eight feet the maximum permitted height in the zoning district by conditional use permit. "Building height" means the vertical distance from the finished grade to the highest point of the structure, excluding chimneys and vents.

² See Section 17.10.060 for additional requirements for accessory structures.

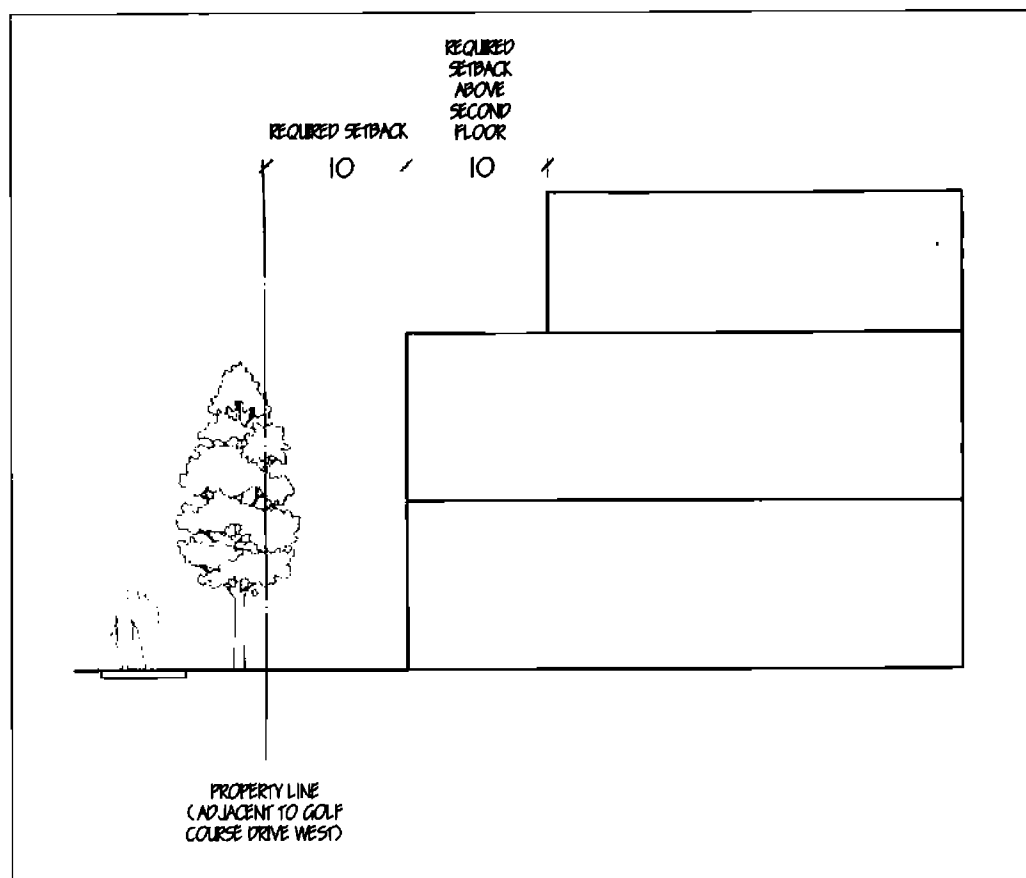
³ Future interior mixed-use streets are publicly-accessible private streets, to be coordinated by the developer at the discretion of the City. For more information, refer to Appendix A: Design Guidelines.

Development Requirement	Commercial-R	Mixed-Use	Industrial
Minimum Rear Setback - Accessory Structure (feet)	5	5	10
Minimum Upper Story Stepback (above second story or 28 feet, whichever is more restrictive) - Golf Course Drive West (feet) ⁴	10	10	N/A
Maximum Floor Area Ratio (FAR) ⁵	0.4	1.5 for commercial and office mixed-use areas; 2.0 for residential uses mixed with office or commercial	0.5

⁴ Upper story stepback is an additional setback beyond the front setback line. *See illustration above.

⁵ Per the Rohnert Park General Plan, FARs for mixed-use classifications are for combined residential and nonresidential development; no separate residential density limitations are specified.

Figure 5-1 UPPER STORY SETBACK ALONG GOLF COURSE DRIVE WEST



SPECIAL PARKING AND ACCESS STANDARDS

The following are special parking standards to promote more pedestrian-friendly development and to discourage providing more parking than is necessary.

SHARED PARKING

Parking can be shared between multiple uses in a mixed-use or commercial environment provided that project proponents show that parking demand will be adequately met at all times and not result in spillover or other parking-related issues. The Planning Director may determine that overall parking requirements can be reduced if parking is shared.

CENTRALIZED PARKING

In mixed-use areas or multi-tenant commercial areas, single parking structures or lots may be used to accommodate several uses within a single area. Shared or central parking facilities like this must be shown to adequately address parking need for a given area thereby reducing the need for dedicated on-site parking for each individual parcel, building, or use.

RECIPROCAL ACCESS AGREEMENTS

Where adjacent parking lots for two developments occur, parking lots should be connected to one another and shared with reciprocal access agreements. The use of parking spaces in this situation shall also be reciprocal.

DEVELOPMENT STANDARDS NOT ADDRESSED

Additional development standards are required that are not provided for in this section. In addition to complying with the development standards above, new development shall comply with the existing Zoning Code in the following areas. In the case of a conflict with the Zoning Code, this Specific Plan's development standards apply.

- Fences, Walls, and Landscaping (*Municipal Code Chapter 17.14 Fences, Walls, and Landscape Standards*)
- Lighting (*Municipal Code Chapter 17.01 Land Use Footnotes/Special Provisions; Municipal Code Chapter 17.27 Signs; and Municipal Code Chapter 17.12 Performance Standards*)
- Required Parking (*Municipal Code Chapter 17.16 Off-Street Parking Requirements*)
- Bicycle Parking (*Municipal Code Section 17.16.140 Bicycle Parking*)
- Signage (*Municipal Code Chapter 17.27 Signs*)
- Industrial Performance Standards (*Municipal Code Chapter 17.12 Performance Standards*)

Chapter Six

CIRCULATION

This chapter describes the circulation improvements proposed for the Northwest Specific Plan Area, including pedestrian, bicycle, and vehicular improvements.

HOW TO USE THIS SECTION

This chapter is intended to provide the reader with an understanding of the traffic related improvements that will be required to accommodate new development in the Specific Plan Area, as well as recommendations for new multi-modal transportation improvements for bicyclists and pedestrians. The reader should use this section to determine which roads will need to change in the future and how these roads will change. The reader should also use this chapter to determine if and how new developments will be expected to contribute to the success of the overall circulation concept for the Northwest Specific Plan Area. Figure 6-1 shows the overall conceptual circulation network plan for the Plan Area and its immediate vicinity, including vehicular, bicycle, and pedestrian facilities and improvements, as described in the following sections.

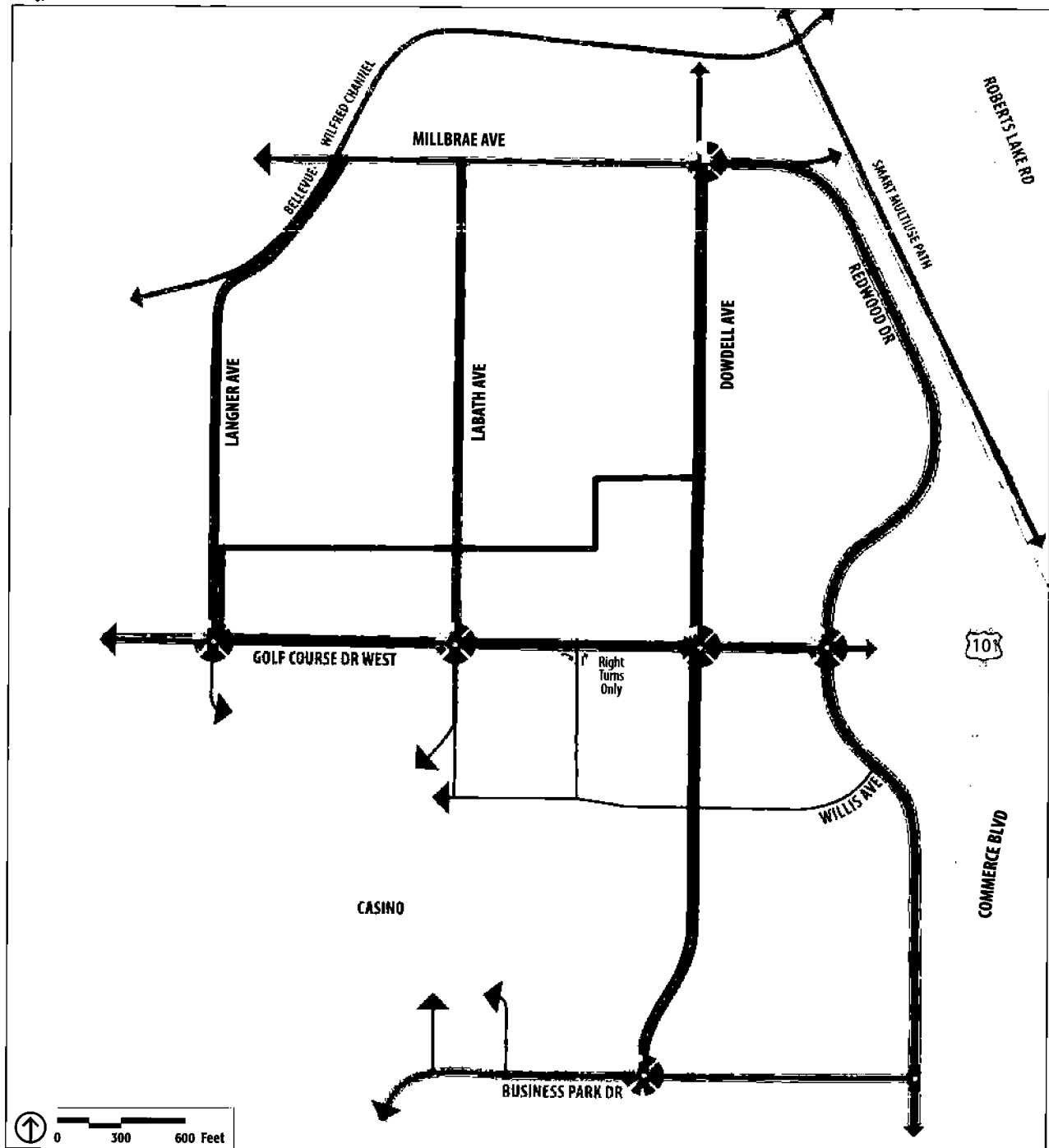
PEDESTRIAN AND BICYCLE CIRCULATION

This section describes major pedestrian and bicycle improvements proposed as part of the Specific Plan. It is important to note that the recommendations for pedestrian and bicycle improvements in this chapter are intended to coordinate and fit in with wider regional efforts. Figure 6-2 shows existing and planned citywide and regional bicycle and pedestrian facilities.

PEDESTRIAN CROSSINGS

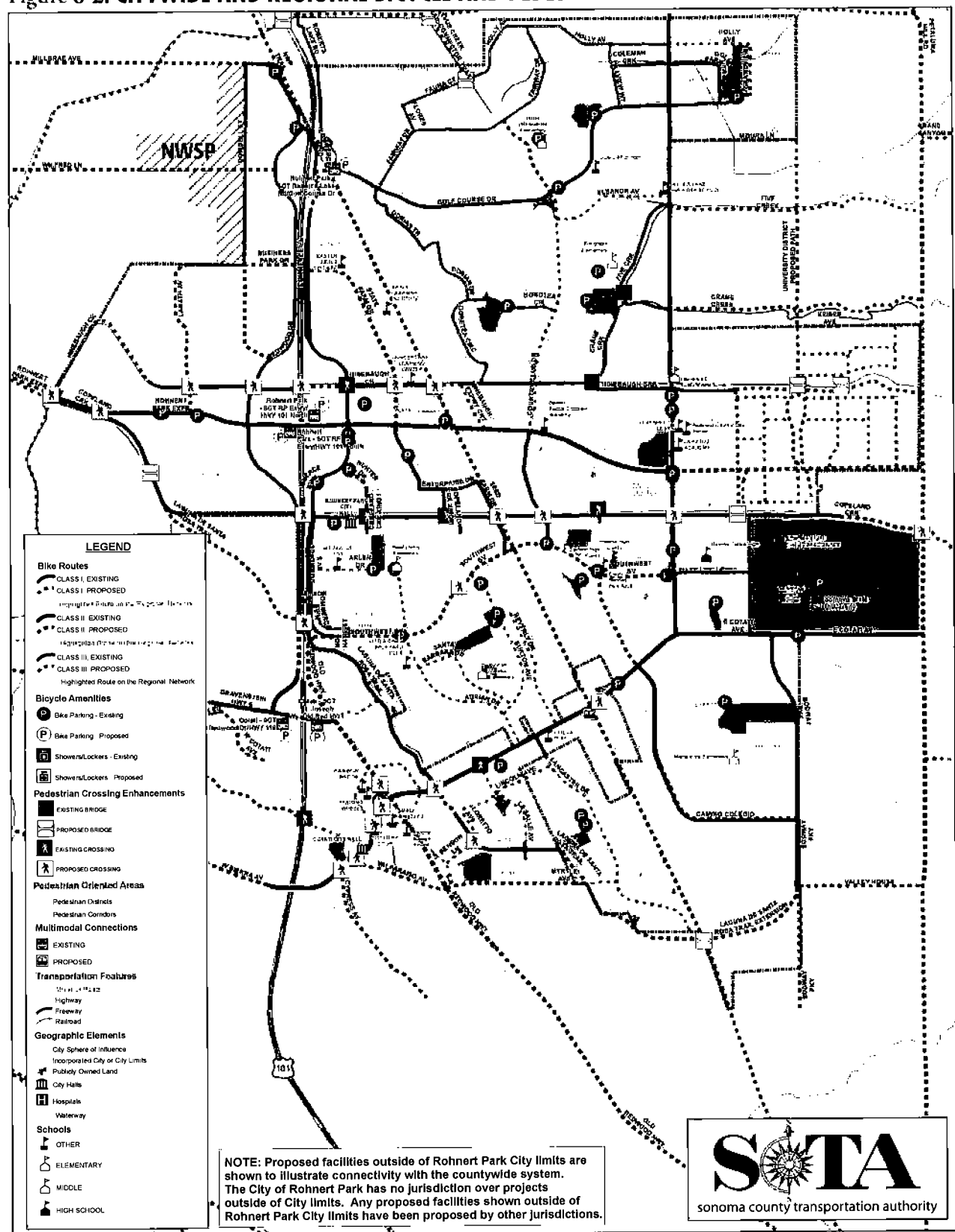
Pedestrian crossings at intersections should be provided and identified at all signalized intersections at a minimum. Crossings may also be appropriate at potential non-signalized intersections or at mid-block locations, if necessary. Crossings can be identified with a variety of methods, including special pavement or various striping techniques, but should be clearly visible to motorists.

Figure 6-1: CIRCULATION PLAN



- | | |
|-------------------------|--------------------------------------------------|
| Northwest Specific Plan | Two Lanes per Direction with Median/Turn Pockets |
| Multi Purpose Trail | One Lane per Direction with Median/Turn Pockets |
| Bike Lanes | One Lane per Direction |
| Bike Route | Intersection Improvement |
| Enhanced Trail Crossing | Traffic Signal |

Figure 6-2: CITYWIDE AND REGIONAL BICYCLE AND PEDESTRIAN FACILITIES



New traffic signal improvements proposed in the Plan Area should provide traffic signalized (protected) pedestrian crossings as appropriate and accommodate bicyclists, ultimately providing easier and safer access. Further discussion of treatments where multi-purpose trails intersect public streets appears below.

ACCESSIBLE PEDESTRIAN FACILITIES

Automobile, pedestrian, and transit facilities within the Northwest Specific Plan Area shall be designed to maintain compliance with Americans with Disabilities Act (ADA). Ramps at crossings should allow for safe access for all users.

BICYCLE CIRCULATION NETWORK

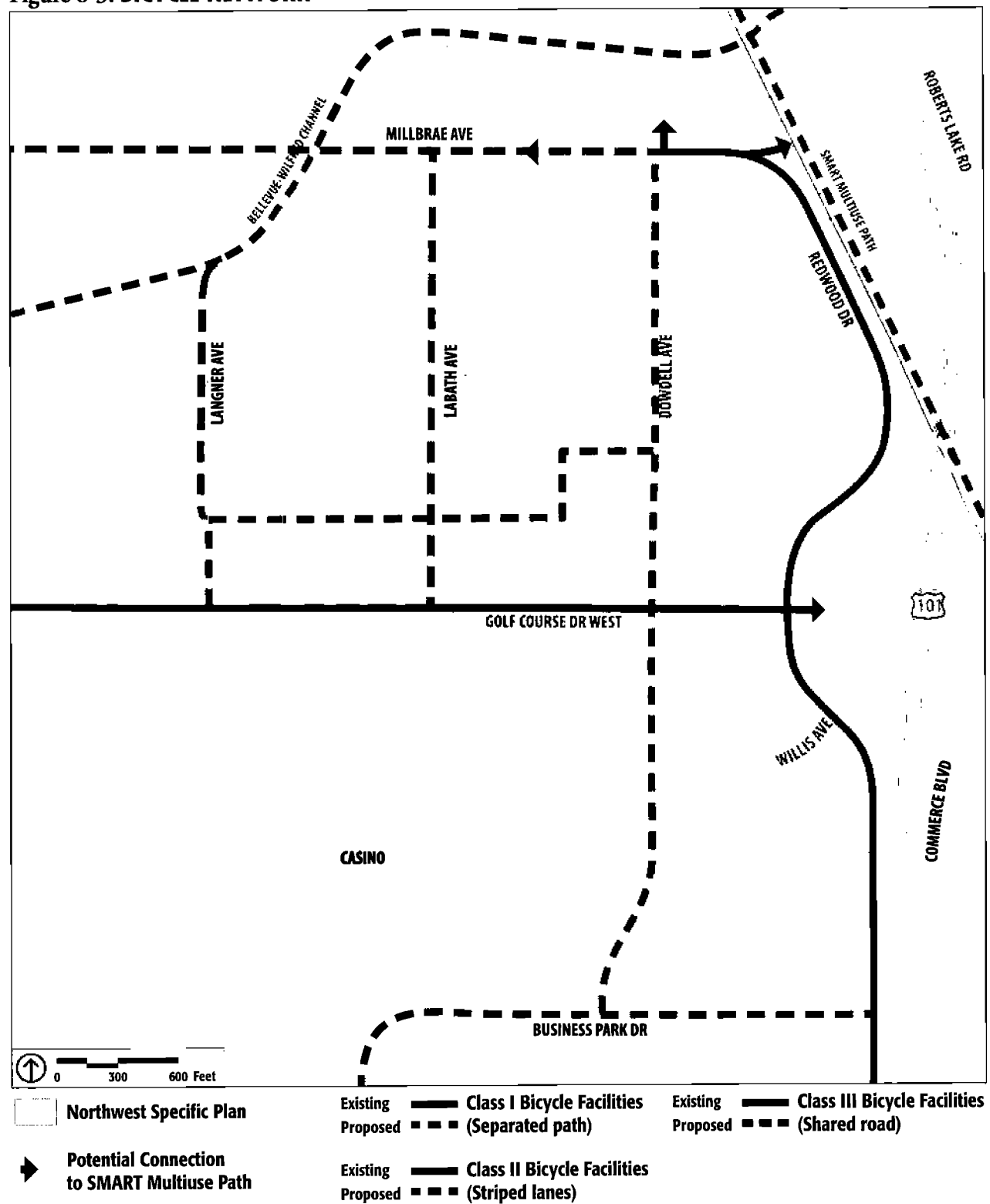
As shown in Figure 6-3, a comprehensive bicycle network is proposed for the Plan Area that would tie into existing and planned bicycle facilities in the Plan Area. Class I bike paths are proposed along the west side of Dowdell Avenue and between Langner Avenue and Dowdell Avenue in the north district. It is proposed that these facilities be provided as multi-purpose trails that can accommodate bicyclists and pedestrians simultaneously and tie into the Class II and Class III facilities proposed throughout the Plan Area as shown on Figure 6-3.

CONNECT TO SMART MULTI-USE PATH

As shown, connections between the bike network proposed and the pending SMART Multi-use Path (MUP) should be established to the fullest extent possible. There are multiple options for achieving this connection, most of which have unique challenges. They are as follows:

- **Connection by Golf Course Drive West.** This connection point is already established since Golf Course Drive West provides on-street Class II bike lanes that will establish clear connections to the SMART MUP on the east side of Highway 101. This connection can be made as soon as the SMART MUP is in operation as there is an at-grade crossing already established.
- **Connection by Millbrae Avenue at Bellevue-Wilfred Channel.** A future Class I bike path is planned along the Bellevue-Wilfred Channel according to the Countywide Pedestrian and Bicycle Master Plan. Once established, this would provide another opportunity to connect bicyclists to the SMART MUP. A significant engineering effort will be needed since it will require a crossing over or under the SMART tracks where the Channel meets them.
- **Connection North of Millbrae Avenue to the Bellevue-Wilfred Channel.** This approach would be similar to the one discussed above, but would provide a more direct connection to the Channel with a new path that extends directly north from the Dowdell Avenue/Millbrae Avenue intersection. This will face similar challenges mentioned above, but will also require a path on private lands that are outside of the Plan Area, north of Millbrae Avenue.

Figure 6-3: BICYCLE NETWORK



- **Connection East of Millbrae Avenue.** This approach would provide a Class I bike path that would extend directly east from the Millbrae Avenue/Dowdell Avenue intersection to connect to the SMART MUP. This may be the most challenging of the four connection options since it would require a new at-grade crossing of the SMART tracks, requiring approval of the California Public Utilities Commission (CPUC).

The City should coordinate with SMART, Sonoma County Transportation Authority, Sonoma County Water Agency, CPUC, and others in planning for potential connections to this important regional bike facility.

EAST-WEST MULTI-PURPOSE TRAIL

It is recommended that new development in the north district incorporate an east-west multi-purpose trail that can accommodate both bicyclists and pedestrians. The proposed trail is shown on Figure 3-2 in Chapter Three. Figure 6-4 shows one potential design for the trail.

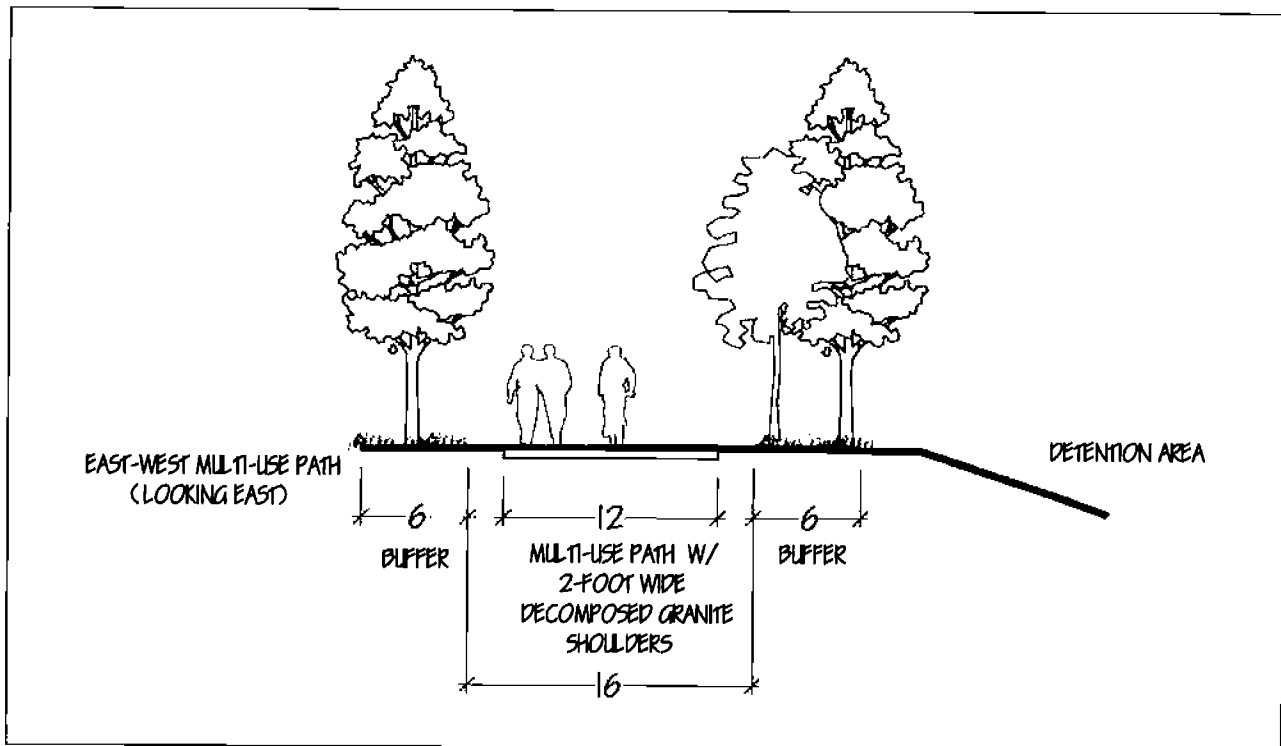


Figure 6-4 TRAIL DESIGN

CLASS I BIKE PATH ON DOWDELL AVENUE AND LANGNER AVENUE

Building on the east-west multi-purpose trail discussed before, it is proposed that new development accommodate a Class I Bike Path connecting Millbrae Avenue to Business Park Drive along Dowdell Avenue (Figure 6-5). This would provide for seamless off-street bicycle connectivity along this north-south street and also tie into the wider bicycle circulation system proposed for the Plan Area. Additionally, it is proposed that a Class I Bike Path be provided along Langner Avenue from the east-west multi-purpose trail to Golf Course Drive West within the existing right-of-way (Figure 6-6), connecting to a future signal at the Langner Avenue/Golf Course Drive intersection.

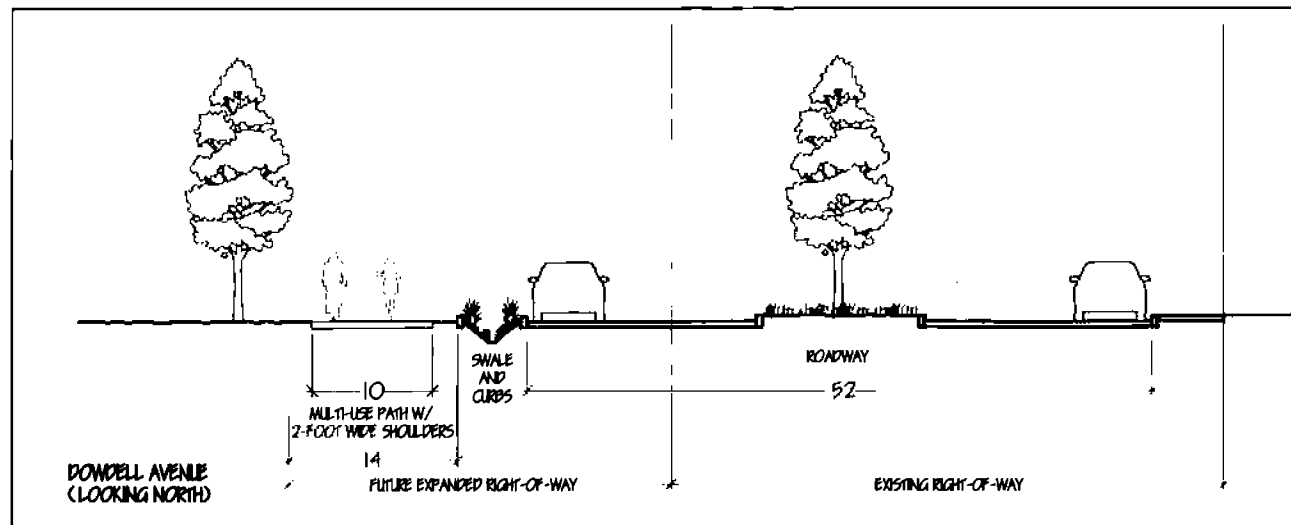


Figure 6-5 DOWDELL AVENUE BIKE PATH

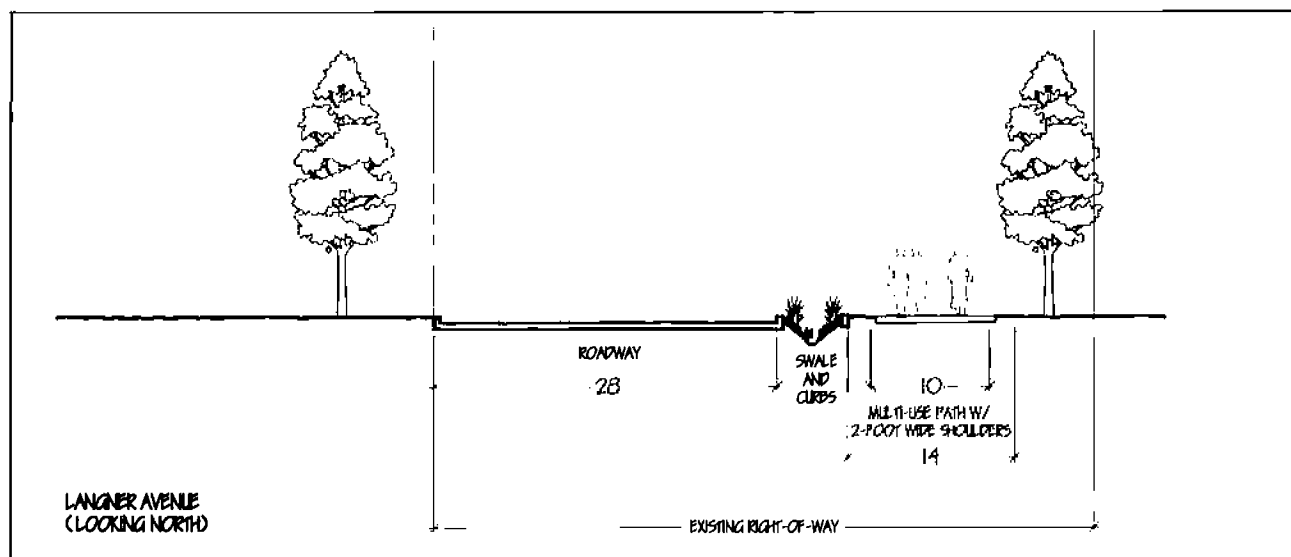


Figure 6-6 LANGNER AVENUE BIKE PATH

CLASS II BIKE LANES ON GOLF COURSE DRIVE WEST, MILLBRAE AVENUE, LABATH AVENUE, AND BUSINESS PARK DRIVE

Class II bicycle lanes have been included in the recent Golf Course Drive West improvements, and are proposed for Labath Avenue, Millbrae Avenue, and Business Park Drive as part of the overall Plan Area bicycle circulation system. This recommendation is consistent with the County-wide Pedestrian and Bicycle Master Plan. Class II bicycle facilities are striped lanes for exclusive use by bicyclists. Bike lanes should be provided at a minimum width of 6 feet.

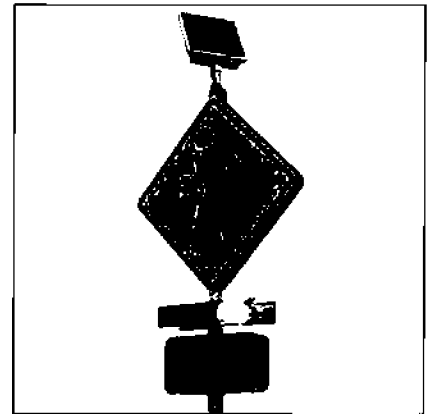
CLASS III BIKE ROUTE ON LANGNER AVENUE

A Class III shared bicycle route is proposed for Langner Avenue connecting from the east-west multi-purpose trail north through the Plan Area and eventually onto Millbrae Avenue. Vehicle traffic volumes on Langner Avenue are projected to remain at low to moderate levels, making effective bicycle circulation possible with the use of a bike route designation, rather than widening to establish designated on-street lanes.

TRAIL CROSSINGS

Trail crossing design is an evolving discipline that should rely on state-of-the-practice design techniques such as those depicted in the *NACTO Urban Bikeway Design Guide*, published by the National Association of City Transportation Officials, as well as the *Guide for the Development of Bicycle Facilities*, published by AASHTO. The ultimate selection of crossing facilities and their configurations should be determined by the City Engineer in consideration of the latest design practices.

- **East-West Multi-Purpose Trail Crossings.** The east-west multi-purpose trail in the north district would cross Labath Avenue and Dowdell Avenue. It is proposed that mid-block crossings be provided on each street to strengthen the pathway network and clearly signify the path crossings. As determined appropriate by the City, the crossings should include features such as high-visibility markings, curb extensions or bulb-outs, and warning lights such as Rapid Rectangular Flashing Beacons (RRFB).
- **Golf Course Drive Crossings.** The Plan includes a multi-purpose path linkage between the western terminus of the east-west multi-purpose path and Golf Course Drive West, at the Langner Avenue intersection. The Plan includes future installation of a traffic signal at this location, which would provide protected crossings of Golf Course Drive for pedestrians and cyclists. Since there would be potential for bicycle and pedestrian crossings of Golf Course Drive West to occur at this location before installation of a signal, a trail crossing should be designed on an interim basis with enhancements (such as a raised median refuge) and possibly warning lights (such as an RRFB system or equivalent).



lent) once a demonstrated pedestrian and bicyclist crossing demand exists.

The Specific Plan depicts a street connecting the mixed-use area in the south district to Golf Course Drive, approximately midway between Labath Avenue and Dowdell Avenue. This intersection would be restricted to right-turns in and out for vehicles, but given the 1,100 spacing between Labath Avenue and Dowdell Avenue, it would be desirable to establish a pedestrian-bicycle crossing on Golf Course Drive West at this location to reduce walking distances and enhance connectivity. The design of this crossing will be somewhat dependent on the final development pattern and level of pedestrian activity that emerges in the Plan Area. The crossing should include a pedestrian-activated signal

The design of the Dowdell Avenue multi-purpose trail crossing of Golf Course Drive West will need to be carefully considered. The design should include added features such as “bike boxes” that allow cyclists to navigate between the multi-purpose trail and the bicycle lanes on Golf Course Drive West. Bike boxes are designated spaces at the limit lines of signalized intersections that allow cyclists to position themselves in front of motor vehicles during a red light, and facilitate movements from a right-side bicycle



Example of Pedestrian Hybrid Beacon Signal. Source: NACTO Urban Bikeway Design Guide, Second Edition, 2012.

lane into a left-turn pocket. The multi-purpose trail crossing of Golf Course Drive West should also include special markings that clearly communicate to drivers that a two-way bikeway is present. Both bike boxes and designated pathway crossings typically make use of green-colored pavement treatments, similar to those installed near the Rohnert Park Expressway freeway interchange. Signal phasing at the inter-

section would also be adjusted as warranted by traffic patterns and/or pedestrian and bicyclist volumes to safely accommodate pedestrian and bicyclist movements.



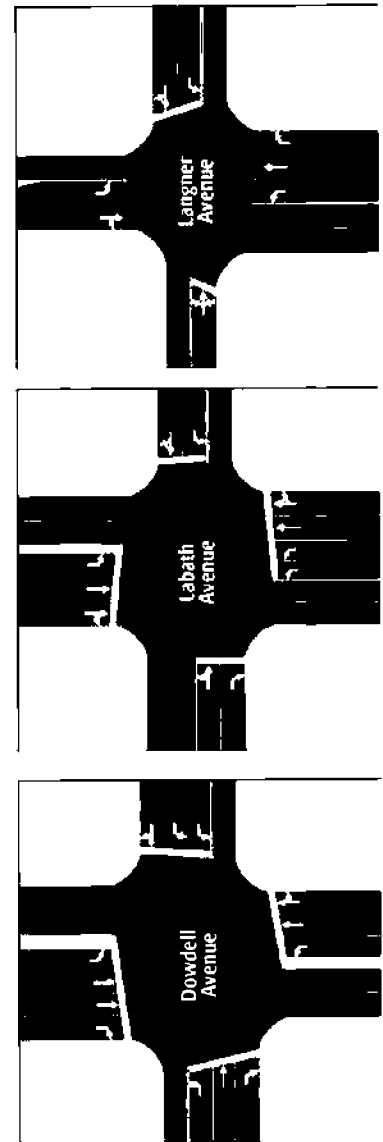
Example of Bike Box.. Source: NACTO Urban Bikeway Design Guide, Second Edition, 2012.

INTERSECTION IMPROVEMENTS

SPECIFIC PLAN INTERSECTIONS

Following is a description of intersection improvements within the Plan Area that shall be completed in order to accommodate future growth. Development adjacent to these intersections shall be responsible for completing frontage roadway widening to accommodate the ultimate intersection “footprints.” The installation of traffic control devices (such as signals) shall occur when deemed by the City Engineer to be warranted in order to accommodate traffic demand. In some cases development may be required to install the traffic control device, and reimbursed by the City for the portion of the cost that is calculated to be beyond the project’s proportional share.

- **Golf Course Drive West/Langner Avenue.** Install a traffic signal when warranted by future growth. Widen the westbound approach to include separate left, through, and right-turn lanes, and widen the eastbound approach to include a left-turn pocket and shared through-right-turn lane. Widen the southbound approach to include a left-turn lane and shared through-right lane.
- **Golf Course Drive West/Labath Avenue.** Modify the Golf Course Drive West/Labath Avenue intersection by widening the westbound approach to include two through lanes and two left-turn lanes. Widen the southbound approach to include a left lane and through-right lane. Signal operations shall be changed to split-phasing on Labath Avenue (northbound and southbound traffic would run during different phases rather than concurrently).
- **Golf Course Drive West/Dowdell Avenue.** Modify the Golf Course Drive West/Dowdell Avenue intersection by widening the southbound approach to include dual southbound left-turn lanes, adding a right-turn pocket with right-turn overlap signal phase on the eastbound approach, and reconfiguring the northbound approach to include separate left, through, and right-turn lanes with a right-turn overlap signal phase. These improvements would be in addition to the widening of Golf Course Drive West to include left-turn pockets and dual through lanes in each direction, which are included in the City’s Public Facilities Finance Plan.

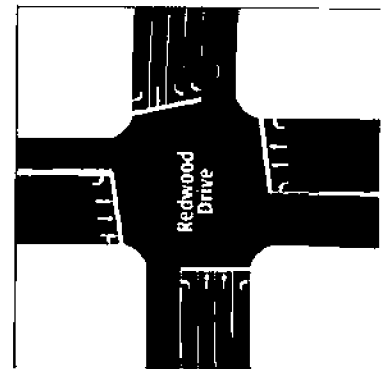


- **Business Park Drive/Dowdell Avenue.** Install a traffic signal when warranted by future traffic growth, and construct an eastbound left-turn pocket. These improvements are included in the 2011 Public Facilities Finance Plan.

OFFSITE INTERSECTIONS

In addition to the intersection improvements described above that are included in this Specific Plan, several intersection improvements outside of the Plan Area are needed to support future growth in the area. Several of the improvements are already included in the City's 2011 Public Facilities Finance Plan, while others will be incorporated into future updates of the Finance Plan. Development within the Specific Plan area shall be responsible for contributing its proportional share of the costs associated with completing the following offsite improvements.

- **Golf Course Drive West/Redwood Drive.** An approved project will construct a southbound right-turn lane at the intersection as part of its frontage improvements. Additional improvements that will be required to support buildout of the Plan and surrounding area include modifying the southbound approach to include dual left-turn lanes and a single through lane (in addition to the new right-turn lane described above), changing the existing eastbound right-turn lane to become a through-right-turn lane (and relocating the bike lane to curbside), constructing a new westbound right-turn pocket on the westbound approach, and adding a right-turn overlap signal phase on the northbound approach.



- **Golf Course Drive West/US 101 South Ramps.** The southbound off-ramp approach shall be restriped to include a through-left-turn lane, through-right-turn lane, and right-turn lane. This modification is included in the 2011 Public Facilities Finance Plan.

STREET STANDARDS

The following roadway improvements shall be incorporated in the design of public streets in the Plan Area. At a minimum, development within the Plan area shall be responsible for constructing frontage roadway improvements that are consistent with the ultimate street configuration as described below, though in some cases the City may require development to complete improvements beyond frontages for operational or safety needs. While sidewalks will be required within the Plan Area on public streets to achieve the community's vision of a pedestrian-friendly environment, they will occur within private property once new development occurs. The new sidewalks will then be acquired into the public right-of-way. Sidewalk requirements are discussed in the following section. Figure 6-7 shows typical sections, including roadways and sidewalks for all streets in the Plan Area.

GOLF COURSE DRIVE WEST

As the Golf Course Drive West Widening Project construction was recently completed, this Specific Plan does not address specific improvements necessary to Golf Course Drive West, except for the following:

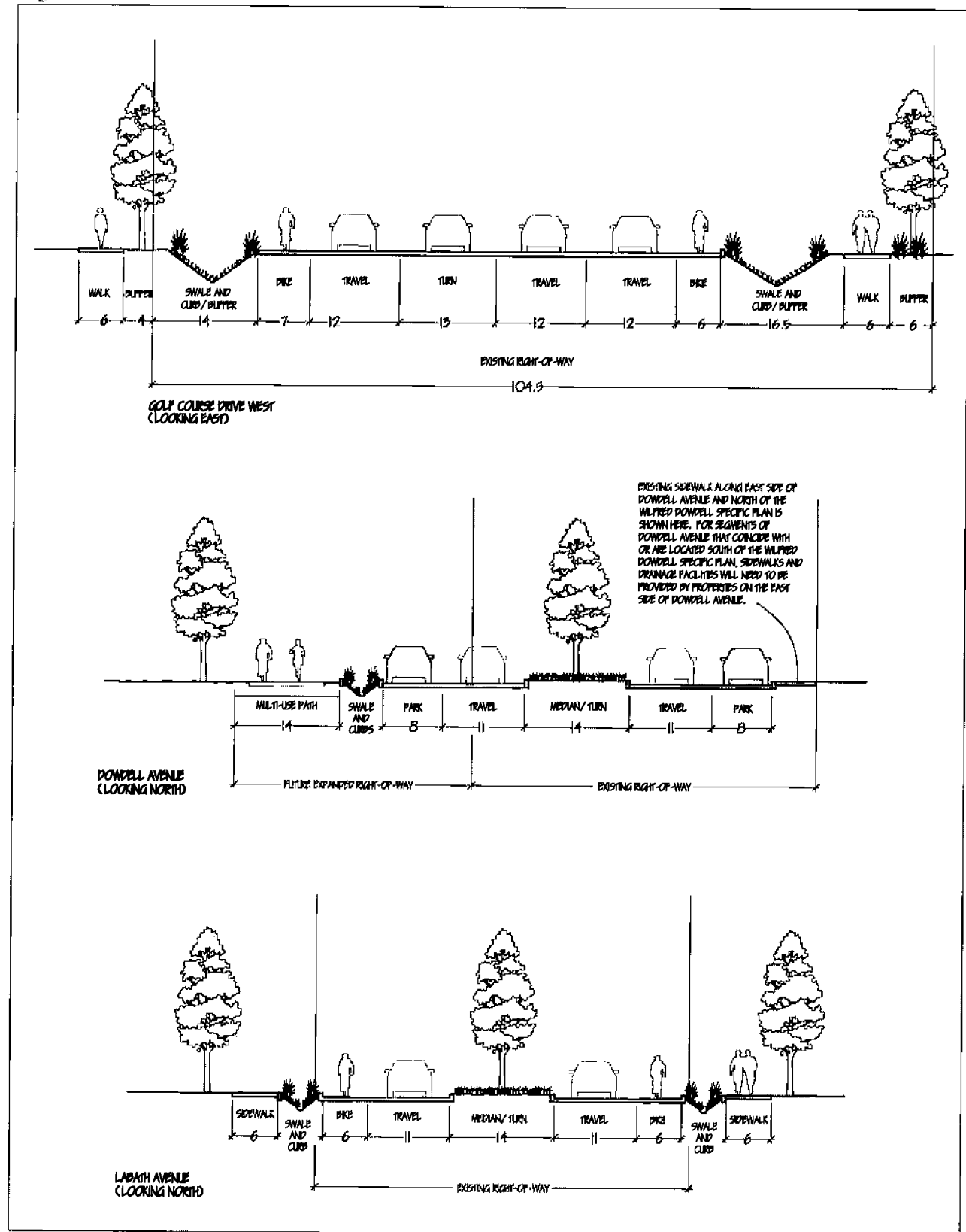
- Golf Course Drive West shall be widened from Labath Avenue to 250 feet east of Labath Avenue to include dual westbound left-turn lanes at buildout of the Specific Plan. Westbound roadway widths shall be as follows: 6-foot bicycle lane adjacent to the curb, two 12-foot through lanes, two 11-foot left-turn lanes, and a 2-foot median. The eastbound widths shall be as follows: 6-foot bicycle lane adjacent to the curb and two 12-foot through lanes. This is a unique recommendation only applicable at the identified location. The cross section shown in Figure 6-7 shows a typical condition.

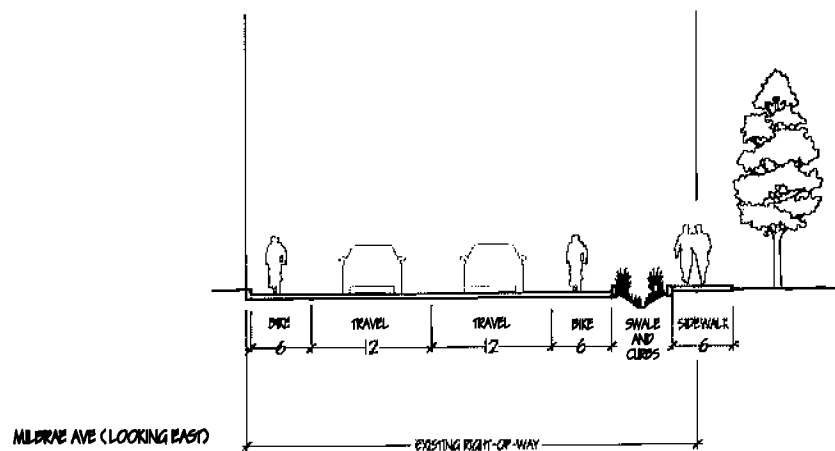
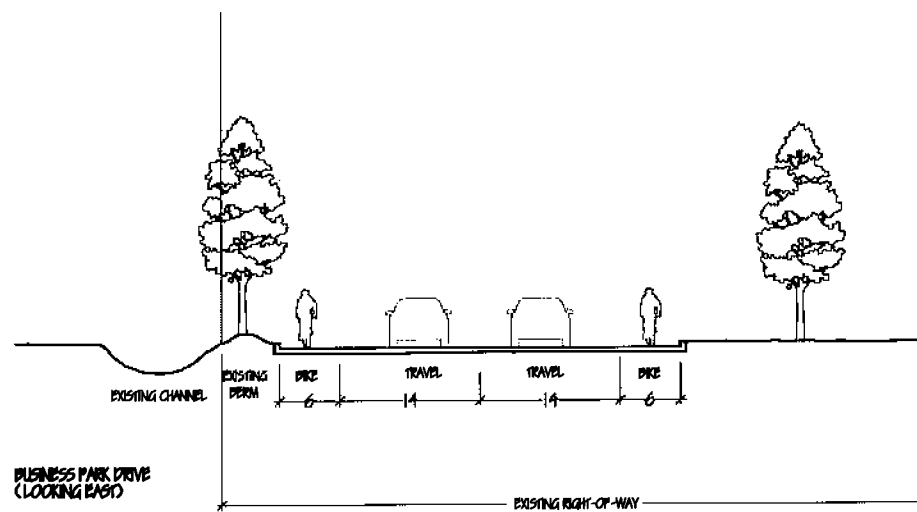
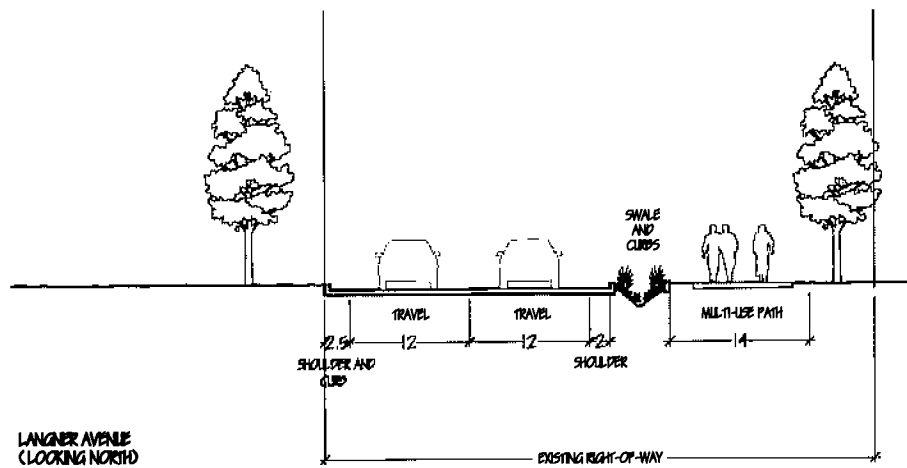
DOWDELL AVENUE

The following roadway improvements shall be implemented on Dowdell Avenue, envisioned as a collector street to facilitate travel to and through the Plan Area. In some cases, these recommendations differ from road segments identified in the 2011 Public Facilities Finance Plan.

- **From Millbrae Avenue to 375 feet north of Golf Course Drive West.** Northbound roadway widths shall include an 8-foot parking lane and an 11-foot through lane. The southbound roadway widths shall mirror the northbound widths, and be separated with a 14-foot median that allows for left-turn pockets provided at Millbrae Avenue as well as any major driveways deemed by the City Engineer to be appropriate for median openings. On the west side of Dowdell Avenue, a swale will need to be provided to treat stormwater runoff from the street. A multi-purpose trail running the north-south distance of the Plan Area shall be constructed along the west side of Dowdell Avenue. Driveways along the west side of Dowdell Avenue shall be minimized and consolidated to the degree possible in order to minimize vehicle conflicts with the multi-purpose trail.
- **From 375 feet north of Golf Course Drive West to Golf Course Drive West.** Northbound roadway widths shall include a 14-foot through lane. Southbound roadway widths shall include an 11-foot through/right-turn, two 11-foot left-turn only lanes cut into the median, and a 5-foot median.
- **From Golf Course Drive West to 150 feet south of Golf Course Drive West.** The northbound roadway shall include a 12-foot right-turn lane adjacent to the curb, an 11-foot through lane, and 11-foot left-turn lane. The southbound roadway shall include a 14-foot through lane, and a 5-foot median.

Figure 6-7: STREET SECTIONS





- **From 150 feet south of Golf Course Drive West to Business Park Drive.** Northbound roadway widths shall include an 8-foot parking lane and an 11-foot through lane. The southbound roadway widths shall mirror the northbound, separated by a 14-foot median that allows for left-turn pockets provided at Business Park Drive as well as any major driveways deemed by the City Engineer to be appropriate for median openings. As in the northern segment described above, a swale will need to be provided on the west side of the street to treat stormwater runoff, and driveways will need to be consolidated in order to minimize vehicle conflicts with the multi-purpose trail.

LABATH AVENUE

Per the General Plan, Labath Avenue is designated as a minor collector within the Plan Area. This plan stipulates roadway widths for Labath Avenue in the Plan Area north of Golf Course Drive West, to accommodate anticipated traffic going into the north district.

- Labath Avenue north of Golf Course Drive West shall consist of the following roadway widths. The northbound roadway shall include a 6-foot bicycle lane and an 11-foot through lane. The southbound roadway shall mirror this configuration, separated by a 14-foot turn lane, or a median if deemed by the City Engineer to be appropriate. Swales will need to be provided along either side of the street to collect and treat stormwater. A 6-foot sidewalk is proposed on either side of the street.

LANGNER AVENUE

The following roadway improvements are recommended for Langner Avenue.

- From the northern Plan Area boundary to 150 feet north of Golf Course Drive West, the roadway widths shall include 12-foot through lanes in each direction. Two-foot shoulders should be provided in each direction adjacent to travel lanes. A swale should be provided on the east side of the street to collect and treat stormwater. A multi-purpose trail is recommended to connect from Golf Course Drive West to the east-west bike path proposed in the north district.
- From 150 feet north of Golf Course Drive West to Golf Course Drive West, the roadway shall include a 13-foot northbound through lane, a 13-foot southbound shared right-turn/through lane, and a 12-foot southbound left-turn only lane. The total curb-to-curb width is 38 feet. Two-foot shoulders should be provided in each direction. A swale should be provided on the east side of the street to collect and treat stormwater.

BUSINESS PARK DRIVE

The following roadway improvements are recommended for Business Park Drive.

- Business Park Drive shall be restriped within the existing roadway dimensions to change to two 14-foot through lanes in each direction, and two 6-foot bicycle lanes in each direction.

MILLBRAE AVENUE

The following roadway improvements are recommended for Millbrae Avenue.

- Millbrae Avenue roadway widths shall include 12-foot through lanes in each direction, and 6-foot bicycle lanes on both sides. A swale should be provided on the south side of the street to collect and treat stormwater. A 6-foot sidewalk should also be provided along the south side of the street.

SIDEWALK IMPROVEMENTS

The following sidewalk improvements reflect the minimum standards that shall be constructed by new development as it occurs. Sidewalks are anticipated to be provided adjacent to the curb line, as discussed in the previous section, and could require right-of-way dedication. The following minimum standards include sidewalk and/or landscaping widths. Refer to Figure 6-7 in the previous section, which shows typical sections, including roadways and sidewalks for all streets in the Plan Area.

GOLF COURSE DRIVE WEST (NORTH SIDE)

The north side of Golf Course Drive West shall include the following sidewalk components:

- 4-foot-wide landscape strip buffer adjacent to the back of the existing or future swale.
- 6-foot-wide sidewalk adjacent to the back of the landscape strip.

DOWDELL AVENUE

Dowdell Avenue shall include the following sidewalk components:

- 14-foot multi-purpose trail along the west side of the street, which would include a 10-foot path and 2-foot decomposed granite shoulders on either side of the path.

Large canopy trees should be planted at regular intervals of 30 feet on center within private development setbacks.

LABATH AVENUE

Labath Avenue shall include the following sidewalk components:

- Drainage swales adjacent to the back of curb.
- 6-foot-wide sidewalks adjacent to the back of the drainage swale

Medium canopy trees should be planted at regular intervals of 20 to 30 feet on center within private development setbacks. The new drainage swales should connect with the existing or future drainage swales along the north side of Golf Course Drive West.

LANGNER AVENUE

Langner Avenue shall include the following sidewalk components:

- Drainage swale adjacent to the back of curb.
- A 14-foot-wide multi-purpose trail adjacent to the back of the swale.

Medium canopy trees should be planted at regular intervals of 20 to 30 feet on center within private development setbacks. The new drainage swale is to connect with the existing or future drainage swales along the north side of Golf Course Drive West.

BUSINESS PARK DRIVE

Improvements to Business Park Drive are intended to occur within the curb-to-curb roadway. No sidewalk improvements are anticipated.

MILLBRAE AVENUE

Millbrae Avenue shall include the following sidewalk components:

- Drainage swales adjacent to the back of curb.
- 6-foot-wide sidewalks adjacent to the back of the drainage swale

Large canopy trees should be planted at regular intervals of 30 feet on center within private development setbacks.

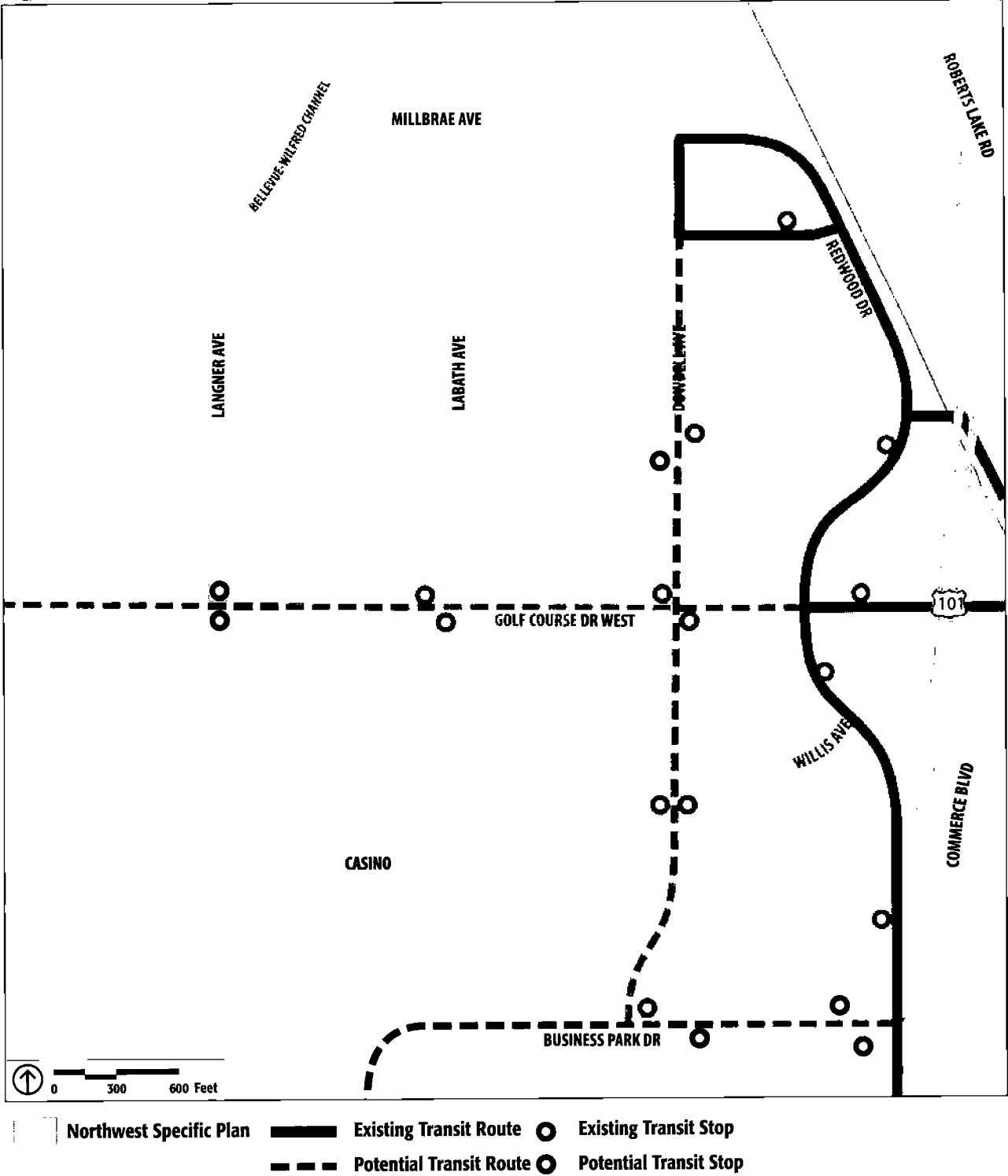
TRANSIT

Sonoma County Transit (SCT) provides local and inter-city service within Rohnert and Sonoma County, with routes that currently pass adjacent to portions of the Plan Area. As future growth occurs, it is likely that SCT will expand service into the Plan Area by altering current bus routes or potentially introducing new bus routes. Current SCT routes run along Redwood Drive, Business Park Drive, Commerce Boulevard, and Golf Course Drive West to the east of Redwood Drive.

Based on the proposed development pattern within the Specific Plan and current transit routes, the streets most likely to accommodate new bus service in the Plan Area are Golf Course Drive West and Dowdell Avenue. Future projects within the Plan Area that include frontages on these streets will need to coordinate with the City and SCT to determine the optimal locations of transit stops, bus shelters, and other transit-supportive amenities.

Figure 6-8 shows the locations of current transit routes and stops, as well as the potential locations of future transit service and new bus stops. New bus stops are shown conceptually on Golf Course Drive West at the intersections of Dowdell Avenue, Labath Avenue, and Langner Avenue, as well as along Dowdell Avenue near the east-west multi-purpose path and near the future extension of Willis Avenue. New stops are also shown on Business Park Drive near the Dowdell Avenue intersection.

Figure 6-8: POTENTIAL TRANSIT NETWORK



CIRCULATION POLICIES

This section provides an overview of circulation policies that shall be followed by private developers and/or the City of Rohnert Park during implementation of this Specific Plan.

MEDIANS

- Policy CIR-1.1 Median breaks on Dowdell Avenue shall be provided only at public intersections and major driveways, as deemed appropriate by the City Engineer.
- Policy CIR-1.2 Center turn lanes and/or turn pockets shall be provided on the segments of Labath Avenue and Langner Avenue within the boundaries of the Specific Plan, accommodating full access to adjacent parcels.
- Policy CIR-1.3 Raised medians with a minimum length of 150 feet shall be constructed, where needed, to restrict left-turn access on approaches to public intersections.
- Policy CIR-1.4 Construct a raised median on Golf Course Drive West within the Specific Plan Area, allowing median breaks with turn pockets only at public street intersections.
- Policy CIR-1.5 Construct a raised median on Dowdell Avenue within the Specific Plan Area, allowing median breaks with turn pockets only at public street intersections and major driveways, as deemed appropriate by the City Engineer. Driveway openings shall be minimized to the degree possible.
- Policy CIR-1.6 Restrict vehicle movements at all driveways within 150 feet of public intersections to right-turns in and out, using raised medians, where deemed appropriate by the City Engineer to reinforce turn prohibitions.

TRANSPORTATION DEMAND MANAGEMENT

- Policy CIR-1.7 Encourage developments within the Northwest Specific Plan Area to reduce traffic demand through transportation demand management (TDM) mechanisms including, but not limited to:
- Implementation and/or coordination of shuttle services to the Graton Rancheria Resort Casino and SMART.
 - Provision of employee transit pass subsidies.

- Creation of enhanced onsite bicycle and pedestrian amenities.
- Commitment to allowing alternative work and telecommute schedules.
- Establishment of employee rideshare programs.

TRANSIT SERVICE

Policy CIR-1.8 The City shall routinely coordinate with and encourage Sonoma County Transit to establish transit routes and stops within the Northwest Specific Plan Area as development occurs.

TRANSIT FACILITIES

Policy CIR-1.9 Individual projects within the Northwest Specific Plan Area shall incorporate bus pullouts, transit shelters, and supporting transit facilities at appropriate locations. Suggested locations are shown on Figure 6-7, though final placement of bus stops and shelters shall be determined by Sonoma County Transit.

STREETS AND INTERSECTIONS

Policy CIR-1.10 The new interior “main streets” proposed to connect through the south district should be designed to create a pleasant, interconnected pedestrian environment.

Policy CIR-1.11 All streets in the Plan Area, whether existing streets that are being redesigned or new streets, should be constructed as “complete streets” that accommodate vehicles, bicycles, pedestrians, transit use, and parking if allowed.

Policy CIR-1.12 All intersections in the Plan Area should be designed so that they are accommodating of pedestrians and bicyclists.

Policy CIR-1.13 Trail and multi-use path crossings of public roadways shall be designed using state-of-the-practice techniques to maintain pedestrian and bicyclist safety, such as those published by the National Association of City Transportation Officials (NACTO) in the *Urban Bikeway Design Guide and Urban Street Design Guide*.

Policy CIR-1.14 New development within the Plan area shall be responsible for funding and constructing all onsite roadway and intersection improvements identified in the Northwest Specific Plan.

Policy CIR-1.15 The City shall update the Public Facilities Finance Plan to incorporate the offsite intersection and roadway improvements identified in the Northwest Specific Plan, allowing future development within the Plan area to contribute its proportional share of the costs associated with these improvements.

Chapter Seven

UTILITIES AND INFRASTRUCTURE

This chapter describes the utility improvements required to implement the vision identified in this Specific Plan. Improvements are identified for site drainage, water, waste water, reclaimed water, and dry utility systems.

HOW TO USE THIS SECTION

The reader should use this section to gain an understanding of the infrastructure requirements that will be necessary to accommodate new growth projected for the Plan Area. The reader should also use this to understand infrastructure-related policies that new development may need to follow.

CONCEPTUAL DRAINAGE PLAN

DESIGN CRITERIA

New development and redevelopment within the Plan Area will result in an increase in impervious surfaces and a reduction in the porous or pervious surfaces. Hydrology changes can include reduced infiltration, increased runoff volume, velocity, increased flow frequency and duration, and less time to reach peak flow.

The design criteria specified in the City of Rohnert Park's *Storm Drain Design Standards* and the Sonoma County Water Agency's (SCWA) *Flood Control Design Criteria* were used in this conceptual drainage study. The modified rational method was used to analyze existing and proposed conditions using the following equation:

$$Q = C * I * A * K$$

Where: Q = flow (cubic feet per second, cfs)

C = composite runoff coefficient (unitless)

I = rainfall intensity for design storm (inches/hour)

A = drainage area (acres)

K = factor for mean seasonal precipitation (unitless)

The runoff coefficient (C) ranges between 0.30 and 0.90, depending on land use, soil characteristics, and slope of the terrain. Table 1.1 of the City of Rohnert Park's *Storm Drain Design Standards* was used to determine existing and proposed runoff coefficients that were weighted in proportion to the areas of each land use. Existing land use was assumed to be approximately 3% impervious to be consistent with the assumptions in the Golf Course Drive Improvements Project and Graton Rancheria hydrology reports. Figure 7-1 shows the location of four tributary areas in the Plan Area. The runoff coefficients used in the analysis for the tributary areas are summarized in Table 7-1; backup documentation is provided in Appendix A.

The rainfall intensity for the 10-year and 100-year storms was computed using the equation provided in the City of Rohnert Park's *Storm Drain Design Standards* as follows:

$$I = 5.12 * Y^{0.1469} * t^{-0.528}$$

Where: I = rainfall intensity in inches per hour

Y = recurrence interval (10 or 100 years)

t = time of concentration (duration in minutes)

The time of concentration was taken from the City of Rohnert Park's *Storm Drain Design Standards* to be 15 minutes for existing conditions (open space or residential with less than two units per acre) and 7 minutes for proposed land uses (commercial, industrial, and residential with more than 8 units per acre). The rainfall intensities used in the calculations are summarized in Table 7-2.

The mean seasonal precipitation (K) factor for this project was determined to be 1, based on Plate B-4 in the SCWA's *Flood Control Design Criteria* manual and that the mean seasonal precipitation in the City of Rohnert Park is approximately 30 inches per year.

EXISTING SITE DRAINAGE

The topographic gradient at the site is generally from east to west, although the Plan Area is so flat that drainage also occurs to the north and south. Stormwater runoff in the area currently is conveyed by overland sheet flow prior to reaching the roadways. There are roadside ditches along both sides of Labath Avenue and Langner Avenue, and along the south side of Millbrae Avenue, north side of Golf Course Drive West, north side of Business Park Drive, and west side of Dowdell Avenue. These ditches convey runoff from the Plan Area with eventual discharge into Bellevue-Wilfred Channel. Additionally, the recent Golf Course Drive West improvements include a subsurface 15- to 42-inch storm drain pipe along the south side of Golf Course Drive West from Dowdell Avenue to the Wilfred-Bellevue Channel, designed to accommodate runoff from new development. The existing drainage patterns for the four tributary areas, as shown on Figure 7-1, are discussed in the following paragraphs.

Table 7-1: RUNOFF COEFFICIENTS

Location	Description	Runoff Coefficient	Area (acres)
Area A	Existing – Agricultural and Rural Residential	0.32	24.7
Area A	Proposed – Mixed Use	0.78	24.7
Area B	Existing – Agricultural and Rural Residential	0.32	9.7
Area B	Proposed – Mixed Use	0.84	9.7
Area C	Existing – Agricultural and Rural Residential	0.32	38.2
Area C	Proposed – Mixed Use/Commercial	0.79	38.2
Area D	Existing – Agricultural and Rural Residential	0.32	18.2
Area D	Proposed– Flex (Industrial and Office)	0.80	18.2

Table 7-2: RAINFALL INTENSITY

Description	Time of Concentration (minutes)	Rainfall Intensity – 10 Year Storm (inches/hour)	Rainfall Intensity – 100 Year Storm (inches/hour)
Existing	15	1.72	2.41
Proposed	7	2.57	3.60

Area A

Area A consists of the southernmost 24.7 acres south of Golf Course Drive West (south district) that drains south into Labath Creek. There are no existing drainage improvements within this area. The casino site to the west is drained via an internal private drainage system that discharges to protected agricultural lands west of the casino.

There is an existing triple 60-inch storm drain system beneath Business Park Drive that conveys runoff from City properties to the east. However, this system is isolated from Labath Creek and is not designed to take runoff from the Specific Plan Area. Runoff from Area A currently is conveyed via overland flow to the south, where it enters Labath Creek or runoff flows into the roadside ditches along Labath Avenue or Dowdell Avenue with eventual discharge into Labath Creek.

Area B

Area B is also located in south district but consists of the northerly 9.7 acres that drains to the north to Golf Course Drive West. Runoff from this area is directed into a bioretention swale that discharges via catch basins to the existing 30-inch to 42-inch storm drain line located along the south side of Golf Course Drive West. This line was installed as part of the Golf Course Drive West improvement project.

The map illustrates the Northwest Specific Plan area, bounded by Millbrae Ave to the north, Hargrave Ave to the west, and Business Park Drive to the south. Key streets include Whistler Ave, Langer Ave, Labath Ave, Dowdell Ave, Willis Ave, and Redwood Drive. The plan identifies four distinct areas: Area A (24.7 ac), Area B (9.7 ac), Area C (38.2 ac), and Area D (18.2 ac). Storm drain infrastructure is shown with various pipe sizes (e.g., 42", 36", 60", 48", 15", 30") and flow directions. Specific features include a ditch, a culvert, and a triple 60" pipe. A note indicates that casino runoff is pumped to agricultural lands to the west. A legend defines symbols for outfall, culvert, ditch, and the Northwest Specific Plan boundary. A scale bar (0 to 1,200 feet) and a north arrow are provided in the bottom left corner.

The storm drain line extends west from Dowdell Avenue to an outfall at the Bellevue-Wilfred Flood Control Channel.

The storm drain beneath Golf Course Drive West was designed to accommodate runoff from the 10-year storm for the 9.7 acres in a fully developed condition. Therefore, the detention requirements for future development in the Plan Area will be to not exceed the peak runoff for the post-development 10-year storm of 20.9 cfs.

Area C

This area is in the north district of the Plan Area and consists of the southerly 38.2 acres that drains south to Golf Course Drive West. The topography in the north district is extremely flat. The differentiation between Area C and Area D is presumed to be the Plan Area boundary line, approximately 700 feet north of Golf Course Drive West.

Existing drainage improvements in this area consist of roadside ditches on both sides of Labath Avenue and Langner Avenue, as well as a ditch along the north side of Golf Course Drive West. Improvements were made to facilitate drainage within this ditch by replacing culverts and removing accumulated sediment as part of the Golf Course Drive West Improvement Project.

Runoff from this area appears to flow in a westerly direction, where it is intercepted by roadside ditches along Labath Avenue or Langner Avenue, which flow to the south and into the ditch along the north side of Golf Course Drive West. Some of the runoff from Area C flows directly to the south and into the ditch along Golf Course Drive West. Runoff in this ditch flows to the west with eventual discharge into the Bellevue-Wilfred Flood Control Channel. At the intersection of Golf Course Drive West and Whistler Avenue, there are three 24-inch cross culverts that equilibrate flows between the ditch on the north side of the street and the enclosed 42-inch storm drain pipe on the south side of the street.

The Golf Course Drive West storm drain line was designed to accommodate flow from the 10-year storm for 13 acres of Area C (not the entire 38.2 acres) in the existing undeveloped condition. Therefore, detention requirements for Area C will be based on not exceeding the pre-development peak runoff of 7.7 cfs for 13 acres, which is what the storm drain system was designed to accommodate.

Area D

Area D is in north district and consists of the most northerly 18.2 acres that drain north to Millbrae Avenue. Runoff from this area flows to the north toward Millbrae Avenue and to the west toward Labath Avenue. Existing drainage facilities include a roadside ditch along the south side of Millbrae Avenue and a roadside ditch along the east side of Labath Avenue, which drains to the north and connects to the Millbrae Avenue ditch. Runoff in the Millbrae Avenue ditch flows to the west and enters an outfall for the Bellevue-Wilfred Flood Control Channel, located approximately 600 feet west of Labath Avenue.

There is also an existing 36-inch to 42-inch storm drain along the east side of Dowdell Avenue in the vicinity of Area D. It expands to a 60-inch pipe just north of Area D and carries runoff to the north, eventually

ally discharging into an outfall for the Bellevue-Wilfred Flood Control Channel. This storm drain was designed to handle flow from areas east of Dowdell Avenue and was not sized to handle runoff from the Plan Area. However, there are several existing inlets on the west side of Dowdell Avenue that connect to lateral pipes that, in turn, connect to the 36-inch storm drain. Future street improvements along the west side of Dowdell Avenue should be able to discharge to this storm drain. However, this system will not be able to accommodate runoff from the future development within Area D.

Existing drainage conditions are shown on Figure 7-1. Calculations were performed to determine peak runoff rates for existing and proposed development conditions for each tributary area. The detailed calculations are provided in Appendix A and summarized in Table 7-3.

Table 7-3: PEAK RUNOFF RATES

Area	Location	Area (acres)	Existing 10-Year Storm (cfs)	Proposed 10-Year Storm (cfs)	Existing 100-Year Storm (cfs)	Proposed 100-Year Storm (cfs)
A	South Half, Drains South to Business Park Drive	24.7	13.5	49.4	18.9	69.3
B	South Half, Drains North to Golf Course Drive West	9.7	5.3	20.9	7.4	29.3
C	North Half – Drains South to Golf Course Drive West	38.2	20.8	77.6	29.2	108.8
D	North Half – Drains North to Millbrae Avenue	18.2	9.9	37.5	13.9	52.6

PROPOSED SITE DRAINAGE

The drainage plan for the Plan Area will preserve the four tributary area boundaries and discharge points described in the previous section. Construction of detention ponds and in some cases off-site drainage improvements will be needed for each tributary area. The Conceptual Drainage Plan describes the applicable design criteria and provides an example of a drainage plan for the Plan Area that would meet the applicable design criteria and preserve the four tributary area boundaries and discharge points characterizing the Plan Area. The location of the detention basins and storm drainage infrastructure is described for conceptual purposes only and the ultimate size and location of the basins and storm drain lines will be determined based on proposed specific development proposals and coordination between landowners. Readers should use the Proposed Site Drainage section of this Conceptual Drainage Plan as an example of the type of system wide improvements that may be considered within the Plan Area.

The proposed project will result in a change in land uses from agricultural and rural residential to mixed use, commercial, and flex, which will result in an increase in impervious surfaces and peak runoff rates

and volumes. Therefore, detention basins, vegetated swales, and/or other drainage improvements will be provided within each tributary area that can contain runoff from the 100-year storm and release it at flow rates that are much less than pre-development conditions. The drainage design criteria and discharge locations are summarized in Table 7-4 and shown on Figure 7-2.

The detention basins for each tributary area were initially sized to contain the difference in runoff between pre- and post-development conditions for the 100-year storm. For Areas B and C, the constraints in runoff rates based on the design of the existing storm drain infrastructure beneath Golf Course Drive West were used to limit discharge from the detention pond to either the 10-year storm for developed conditions (Area B) or 10-year storm for 13 acres in the undeveloped condition for Area C. The computer model HydroCAD was then used to fine tune the detention pond acreages and outlet pipe sizes. Each detention pond was designed to have a maximum pond liquid level of approximately 1.5 feet, a freeboard of 1.0 foot, and side slopes of 3:1. Also, the Santa Rosa Stormwater Calculator was used to determine the treatment area required to capture 100 percent of the 85th percentile, 24-hour storm event. For this analysis, it was assumed that no low impact development (LID) features were incorporated into Plan Area design plans. The results are provided in Appendix A and summarized in Table 7-5. The following paragraphs discuss the design for each tributary area in further detail.

Table 7-4: DETENTION DESIGN CRITERIA AND DISCHARGE LOCATIONS

Area	Design Criteria – 10-Year Storm	Base Flow (cfs)	Design Criteria – 100-Year Storm	Base Flow (cfs)	Discharge Location
A	No Net Increase in Runoff	13.5	No Net Increase in Runoff	18.9	Labath Creek at Business Park Drive
B	No Net Increase in Runoff	20.9 ^a	No Net Increase in Runoff	20.9 ^a	Existing 42-inch storm drain beneath Golf Course Drive West at Labath Avenue
C	No Net Increase in Runoff	7.7 ^b	No Net Increase in Runoff	29.2	Existing drainage ditch on north side of Golf Course Drive West at Langner Av- enue
D	No Net Increase in Runoff	9.9	No Net Increase in Runoff	13.9	Existing drainage ditch on south side of Millbrae Ave- nue and then via storm drain extension in Millbrae Ave- nue West to Bellevue- Wilfred Flood Control Channel

^a Base flow for Area B is set at 20.9 cfs (the post-development runoff for the 10-year storm) since the existing storm drain pipe in Golf Course Drive West was sized to handle runoff from the 9.7 acres in Area B in a fully developed condition.

^b Base flow for Area C is limited to 7.7 cfs, which is the existing flow from the southerly 13 acres of Area C, based on design of the storm drain system in Golf Course Drive West to handle runoff for 13 acres for the 10-year storm in an undeveloped condition.

Figure 7-2: PROPOSED SITE DRAINAGE

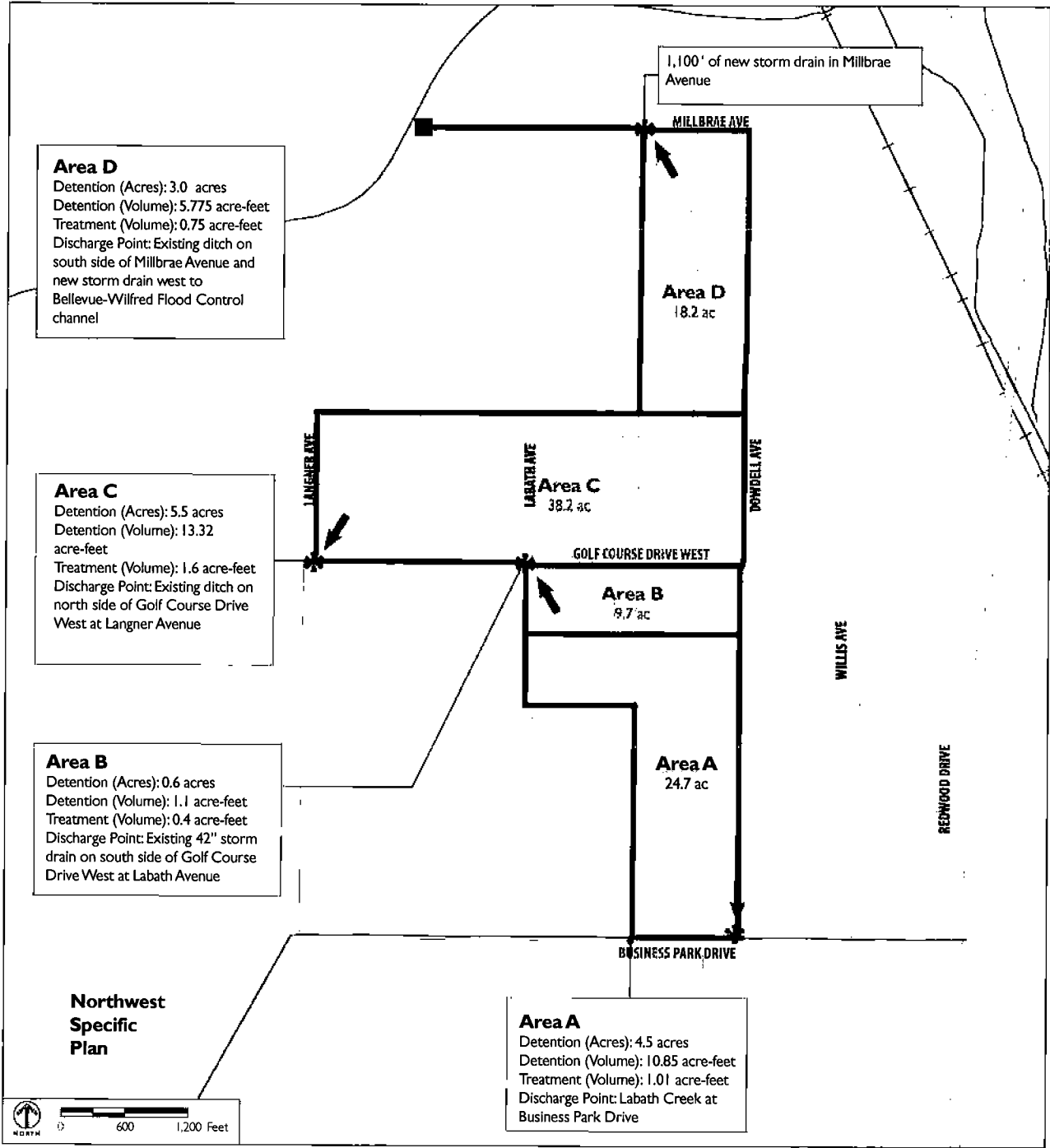


Table 7-5: DETENTION POND SIZE AND TREATMENT REQUIREMENTS

Area	Detention (acres)	Detention (acre-feet)	LID Treatment (acre-feet)
A	4.5	10.85	1.01
B	0.6	1.1	0.40
C	5.5	13.32	1.60
D	3.0	5.775	0.75

Area A

Area A will drain to a detention basin, which is sized with a surface area of 4.5 acres, a storage volume of 10.85 acre-feet, a maximum depth of 1.6 feet, and a freeboard of 1 foot. The discharge pipe was sized to be 14 inches in diameter and is planned to discharge the runoff at a maximum rate of 5.15 cfs to Labath Creek on the north side of Business Park Drive. This is well below the calculated peak flow rate of 13.5 cfs for the 10-year storm under pre-development conditions.

The Dowdell Avenue extension from Business Park Drive north to Golf Course Drive West will require a culvert or bridge crossing of Labath Creek. Improvements along Dowdell Avenue will require a storm drain system to convey street runoff. In addition, a portion of the Wilfred/Dowdell Village Specific Plan area east of Dowdell Avenue will also drain southerly toward Labath Creek. Design of the Area A detention and drainage system will need to be coordinated with both the Dowdell Avenue improvements and also the Wilfred/Dowdell Village Specific Plan drainage. The discharge points for all three systems should be consolidated at a single outfall into Labath Creek, adjacent to the Dowdell Avenue culvert/bridge. This will allow the work in Labath Creek to be coordinated into a single set of resource agency permits and avoid duplication of efforts.

Area B

Area B will drain to a detention basin, which is sized with a surface area of 0.6 acres and a storage volume of 1.1 acre-feet. The maximum depth is approximately 1.6 feet with a freeboard of 1 foot. The discharge pipe was sized to be 20 inches in diameter and is planned to discharge the runoff, which is limited to less than 20.9 cfs based on the existing storm drain design, to the existing 42-inch storm drain on the south side of Golf Course Drive West. The detention basin will have a maximum outflow rate of 9.3 cfs, which is well below the limit of approximately 21 cfs. Subsurface detention strategies should be considered as long as it is keeping with a high quality aesthetic character along Golf Course Drive West.

Area C

Area C will drain to a detention basin, which is sized with a surface area of 5.5 acres, a storage volume of 13.32 acre-feet, a maximum depth of 1.6 feet, and a freeboard of 1 foot. Discharge will be to the existing drainage ditch along the north side of Golf Course Drive West.

The Golf Course Drive West drainage system was designed to handle only the southerly 13 acres of the 38.2 acres within Area C. Therefore, in order to avoid overloading the ditch during a 10-year storm, detention will need to be provided with a discharge not to exceed 7.7 cfs, which is the existing discharge rate from the southerly 13 acres in an undeveloped condition. The detention basin has been designed with a 18-inch diameter culvert, which will result in a maximum outflow of 7.7 cfs during the 10-year storm.

For the 100-year storm, flows are presumed to overtop the ditch and the goal is to protect downstream properties. Therefore, the detention for the 100-year storm is designed to use flows from the entire 38.2 acres in an undeveloped condition as the base flow. According to the calculations provided in Appendix A, the peak discharge for the 100-year storm for Area C under existing (pre-development) conditions is 29.2 cfs.

Area D

Area D will drain to a detention basin, which is sized for a surface area of 3.0 acres, a storage volume of 5.8 acre-feet, a maximum depth of 1.6 feet, and a freeboard of 1 foot. Discharge of runoff from Area D will be to the existing ditch on the south side of Millbrae Avenue. The discharge pipe was sized to be 12 inches in diameter with a maximum outflow of 4.0 cfs.

Runoff from Area D may be conveyed via a drainage system within the Plan Area and/or a new drainage system along the west side of Dowdell Avenue. However, this system will be isolated from the existing 36-inch storm drain pipe on the east side of Dowdell Avenue, which is conveying runoff from areas east of Dowdell Avenue and is currently at capacity.

The existing Millbrae Avenue ditch is presumed to be hydraulically inadequate and too shallow to allow gravity discharge of flow from Area D. Construction of a closed storm drain system in Millbrae Avenue from Area D to a new outfall into the Bellevue-Wildred Flood Control Channel will most likely be needed. Construction of the storm drain system and outfall will require approvals from the County of Sonoma and the Sonoma County Water Agency as well as permitting by the resource agencies.

PROPOSED STREET DRAINAGE

The following recommendations address stormwater strategies to manage new rainwater runoff that will occur with the expansion of roads identified in this Specific Plan. Rainwater runoff from streets will be treated by swales between the street and sidewalk. Overflow will be drained into parallel pipes along the following street segments:

- The north side of Golf Course Drive West should contain matching and mirroring swale and stormwater conveyance mechanisms as the existing south side.
- The west side of Dowdell Avenue should contain a swale between the road and sidewalk for treatment. Stormwater overflow should drain under the road and feed the existing storm drain on the east side of the road. In the south district, a new storm drain will have to be constructed. South of Golf Course Drive West, potential swales on the east side of Dowdell will need to be coordi-

nated with the Wilfred/Dowdell Village Specific Plan.

- Both sides of Labath Avenue and the east side of Langner Avenue should contain swales between the road and sidewalk for treatment, and stormwater should drain into parallel pipes that feed into the Golf Course Drive West conveyance system. These swales should be sized to contain the street stormwater flow from the 100-year storm.
- The south side of Millbrae Avenue should also contain a swale between the road and sidewalk for treatment. This swale should be sized to contain the street stormwater flow from the 100-year storm.

STORMWATER RUNOFF TREATMENT

The City of Rohnert Park has adopted the City of Santa Rosa and County of Sonoma *Storm Water Low Impact Development Technical Design Manual* (LID Manual, 2012) to address stormwater quality and quantity from new development or redevelopment projects. The “design goal” requires that 100 percent of the 85th percentile, 24-hour storm event runoff generated from the developed site be captured and retained on-site to be infiltrated or reused. The required acreage for stormwater runoff treatment for each tributary area is presented in Table 7-5. The acreage was determined using the Santa Rosa Stormwater Calculator.

The Specific Plan Conceptual Land Use Plan does not allocate land for stormwater runoff treatment measures. Treatment measures should be incorporated into the developed portions of the site, using best management practices (BMPs) such as vegetated swales incorporated into landscaping, flow-through planters to treat roof runoff, previous pavement, and other measures that are specified in the LID Manual. Final land use plans will need to confirm that adequate areas are provided for treatment.

DETENTION BASIN LOCATION

Detention basins and a lineal detention swale have been incorporated into the Specific Plan land use plan. The basins and swales are located within Areas A, C, and D. The aggregate acreage for the detention facilities is in accordance with the detention requirements specified in Table 7-5. (However, additional acreage will need to be set aside for a detention facility of 0.6 acres in Area B. This is assumed to be in the form of a subsurface detention facility and will ultimately depend on further study and design. The subsurface detention facility is not shown in the Illustrative Land Use Plan in Chapter 3.) Final drainage design for each area will need to preserve existing flow boundaries and discharge points. Final design of each area may require shifting of the detention facilities depending on the topography and/or the order of development of the individual parcels within each area.

The City of Rohnert Park does not allow pumping in conjunction with private drainage facilities. Final detention facility and drainage system design, as well as site grading, will need to allow for gravity discharge of all stored runoff. Fill may be needed to elevate the onsite drainage systems above backwater effects and to allow gravity drainage. The backwater effect of downstream drainage facilities has not been addressed in this preliminary design document. It can be presumed that flows in both Labath Creek and

the Bellevue-Wilfred Flood Control Channel will be near the top of the banks for both facilities during a 10-year storm.

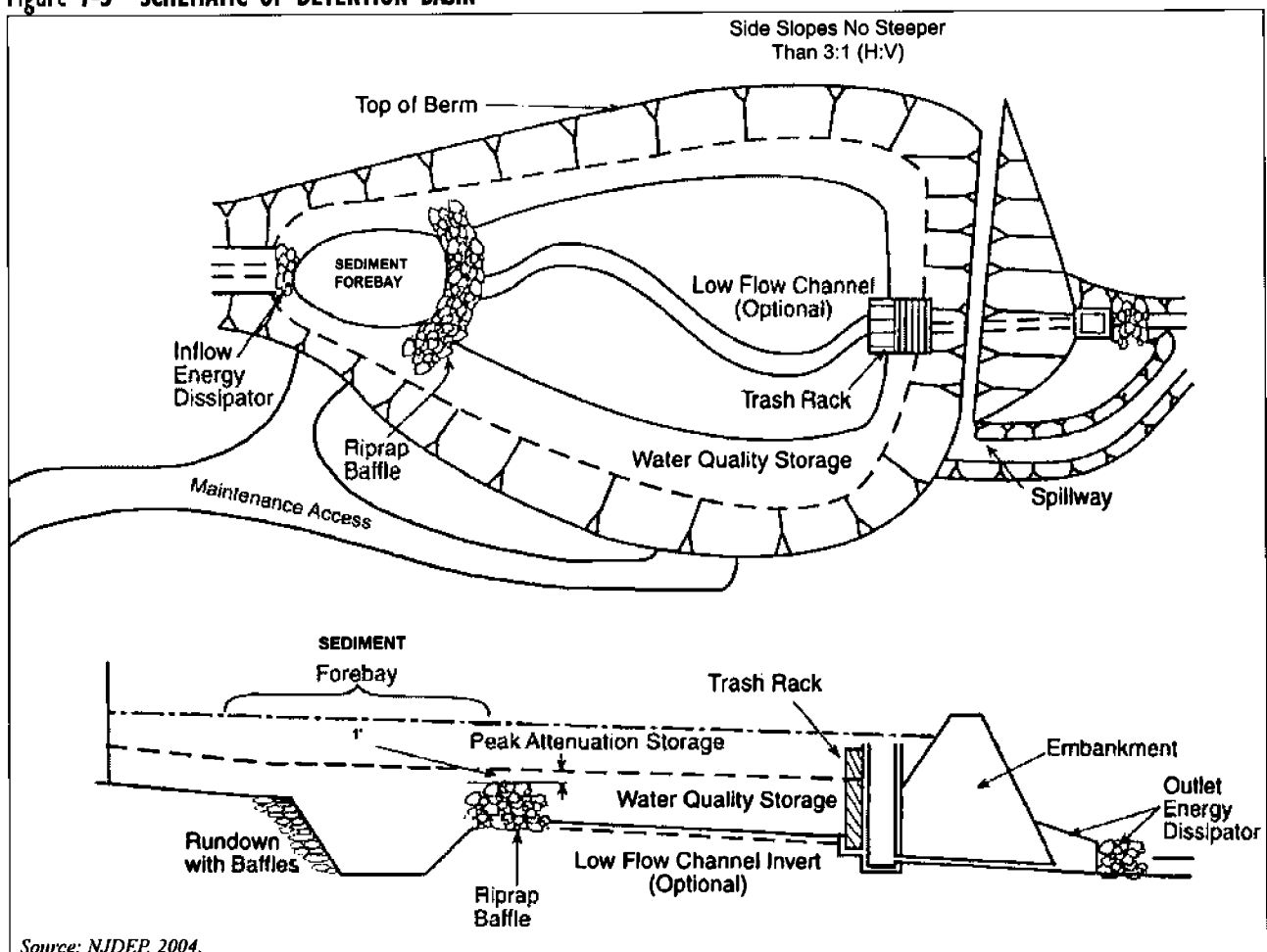
DETENTION BASIN DESIGN

The detention basin for each tributary area is designed to reduce impacts on existing drainage facilities by reducing post-development peak discharges to resemble pre-development peaks. The detention basins are also designed to drain within 48 hours to minimize vector control issues. A schematic of an extended detention basin and its design features is provided in Figure 7-3.

The following features should be incorporated into the design of the detention basins:

- **Basin Shape.** Always maximize the distance between the inlet and outlet. A longer flow path from inlet to outlet will minimize short circuiting and improve the reduction of total suspended solids (TSS). It is preferable to have a length to width ratio of no less than 2:1.
- **Basin Side Slopes.** Basin side slopes should be stable and gentle to facilitate maintenance and access. Side slopes should be no steeper than 3:1 for grass stabilized slopes. The size of the detention ponds for each tributary area assumed the side slopes would be 3:1.
- **Inlet.** Flow energy at concentrated points of inflow should be dissipated. This will limit erosion and promote particle sedimentation. Energy dissipation should be included in the inlet design to reduce re-suspension of accumulated sediment.
- **Forebay.** The forebay provides an opportunity for larger particles to settle out in an area that can be easily maintained and is an optional component of the EDB design. The length of the flow path through the forebay should be maximized and the slope minimized to encourage settling. The forebay outlet should be sized to release 2 percent of the undetained peak 100-year discharge by way of a wall/notch or berm/pipe configuration.
- **Trickle Channel.** Low flows should be conveyed from the forebay to the micropool with a trickle channel. The trickle channel should have a minimum flow capacity equal to the maximum release from the forebay outlet. A concrete trickle channel will help to establish the bottom of the basin and also facilitate regular sediment removal. It can be a V-shaped concrete drain pan or a concrete channel with curbs. A flat-bottomed channel facilitates maintenance. A slope between 0.4 percent and 1 percent is recommended to encourage settling while reducing the potential for low spots. A soft-bottom trickle channel can provide an alternative to concrete. However, maintenance of soft bottom trickle channels require mechanical removal of sediment and vegetation and can provide mosquito habitat. It is recommended that soft bottom trickle channels be designed with a consistent longitudinal slope from forebay to micropool and that they not meander. The recommended minimum depth of a soft bottom trickle channel is 1.5 feet to limit the potential for wetland growth.
- **Micropool.** Although many detention basins incorporate micropools in their design, CASQA guidance recommends that micropools not be included because of vector concerns.

Figure 7-3 SCHEMATIC OF DETENTION BASIN



Source: NJDEP, 2004.

- **Outlet Structure.** The outlet should be designed for complete drawdown of the water over a 48-hour period. The outflow structure can be equipped with a valve so that discharge from the basin can be halted in case of an accidental spill within the watershed. The valve also could be used to regulate the discharge from the basin. Alternatively, the outlet pipe may be sized to limit flow from the detention basin or an orifice plate can be used. A trash rack or screen of sufficient size should be installed at the outlet to prevent clogging at the entrance of the outlet pipe. The trash rack should be sized so it does not interfere with the hydraulic capacity of the outlet pipe.
- **Vegetation.** Vegetation provides erosion control and sediment entrapment. Basin bottom, berms, and side slopes should be planted with turf grass. Xeric grasses with temporary irrigation are recommended to reduce maintenance requirements.
- **Access.** Provide appropriate maintenance access to the forebay and outlet works. For larger basins, this may require stabilized access for maintenance vehicles. Routine maintenance activities for detention basins are often thought to be sediment and trash removal. However, over 90 percent of the total maintenance activities are related to vegetation management and routine mowing. Typical maintenance activities are as follows:

- Schedule semi-annual inspections for the beginning and end of the wet season for standing water, slope stability, sediment accumulation, trash and debris, and presence of burrows.
- Remove accumulated trash and debris in the basins and around the outlet pipe during semi-annual inspections. The frequency of this activity may be altered to meet site-specific requirements.
- Trim vegetation at the beginning and end of the wet season and inspect monthly to prevent establishment of woody vegetation and for aesthetic and vector control reasons.
- Remove accumulated sediment and regrade about every 10 years or when the accumulated sediment volume exceeds 10 percent of the basin volume. Inspect the basin every year for accumulated sediment volume.

VEGETATED SWALE

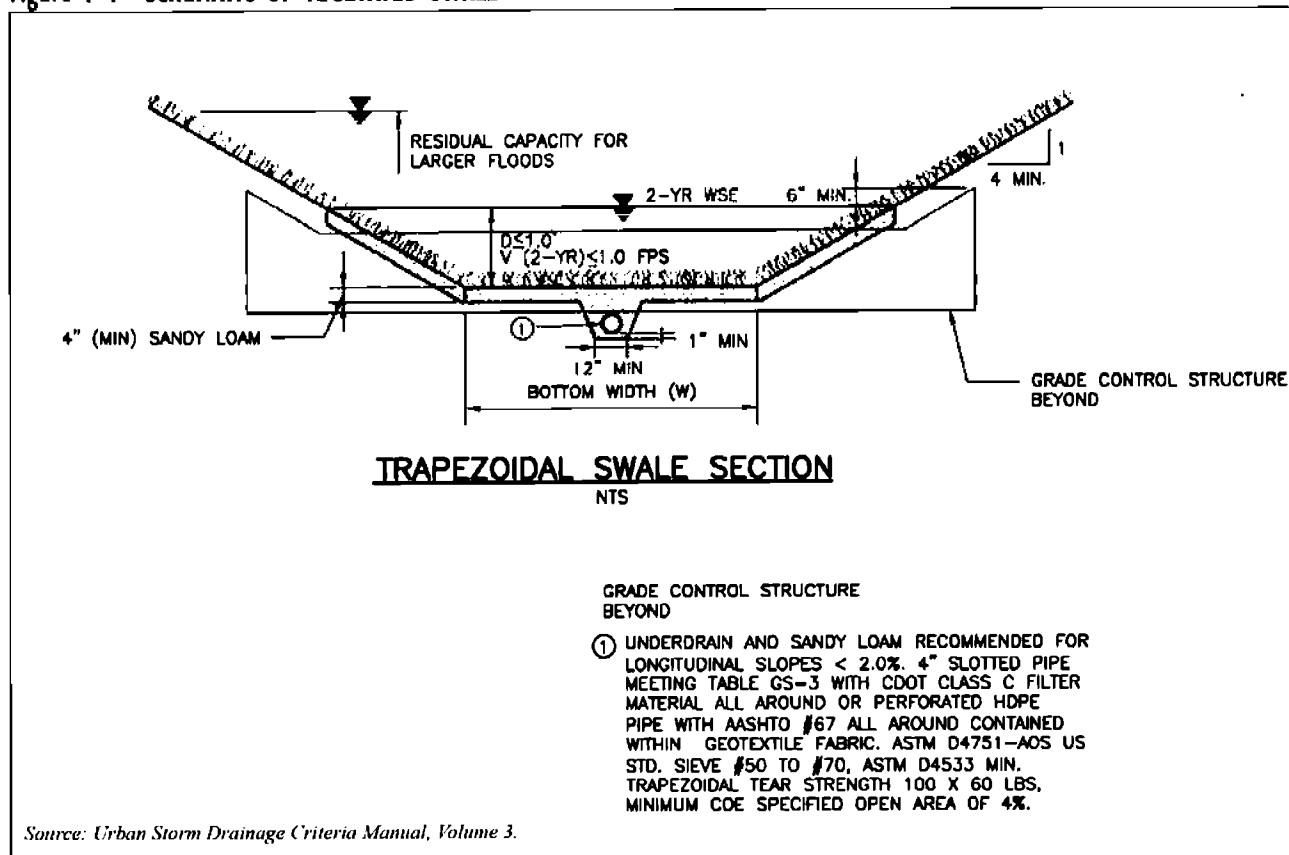
Vegetated swales are open, shallow channels with vegetation covering the side slopes and bottom that collect and slowly convey runoff to downstream discharge points (in this case, into a detention pond). Vegetated swales can be designed to convey high flow as well as water quality flow. The longest flow path for the swale shall have a minimum retention time of 12 minutes for conditions when the treatment flows enter the vegetated swale uniformly along the swale length. Longitudinal slopes of not less than 1 percent and trapezoidal channels are recommended.

A diverse selection of low-growing plants that thrive under specific site, climate, and watering conditions should be specified. Vegetation whose growing season corresponds to the wet season is preferred. Drought tolerant vegetation should be considered for swales that are not part of a regularly irrigated landscaped area. The maximum treatment width of the vegetated swale should be no greater than 10 feet and side slopes should be no greater than 2:1. A schematic of vegetated swale and its design features is provided in Figure 7-4.

Maintenance for vegetated swales should include the following:

- Mow grass to maintain a height of 3 to 4 inches. Litter should always be removed prior to mowing. Clippings should be composted.
- Irrigate during dry weather, or when necessary, to maintain the vegetation.
- Provide weed control, as necessary, to control invasive species.
- Remove obstructions, litter, branches, rocks, trash, and other debris from vegetated swale.
- Repair any damaged areas within the channel identified during inspections. Erosion rills or gullies should be corrected, as needed. Bare areas should be replanted as necessary.

Figure 7-4 SCHEMATIC OF VEGETATED SWALE



- Plant an alternative grass species if the original grass cover has not been successfully established. Reseed and apply mulch to damaged areas.
- Remove all accumulated sediment that may obstruct flow through the swale. Sediment accumulating near culverts or in channels should be removed when it builds up to 3 inches at any spot, or covers vegetation, or once it has accumulated to 10 percent of its original design volume. Replace the grass areas damaged in the process.
- Rototill or cultivate the surface of the sand/soil bed of dry swales, if the swale does not draw down within 48 hours.
- If ponding is observed, grading will be required to restore positive drainage.

Inspection activities include the following:

- **Post-construction.** Inspect after seeding and after the first major storm for any damage.
- **Monthly.** Look for obstructions and trash, draining of ponded flow within 48 hours after a rainfall event, and condition of grasses. Inspect for signs of erosion, damage to vegetation, channelization of flow, and areas of sediment accumulation.

WATER PLAN

A majority of the Plan Area is primarily undeveloped. Water utilities to serve the existing residents and businesses are not present, as these parcels are served by individual wells. Once developed, the area will be served by the City, with wholesale service provided by the Sonoma County Water Agency (SCWA). This section briefly discusses the three facets that are included in a water system: supply/production/treatment, conveyance/storage, and transmission/distribution; the estimated water demand for the Plan Area; proposed water system improvements for the Plan Area; and estimated costs. For the purposes of the utility assessment and recommendations, the Plan Area has been divided into four areas as shown in Figures 7-5 and 7-6. Development assumed to occur in each area is as follows:

- **Area 1.** Flex usage, consisting of 75 percent light industrial and 25 percent office (218,000 square feet).
- **Area 2.** Mixed-use and commercial usage, consisting of 78 multi-family dwelling units, approximately 60 percent retail and 40 percent office (136,000 square feet).
- **Area 3.** Mixed use and commercial usage, consisting of 105 multi-family residential units and 134,000 square feet retail.
- **Area 4.** Mixed use, consisting of 205 multi-family dwelling units, a 100 room hotel, and 247,000 square feet retail.

SUPPLY/PRODUCTION/TREATMENT

There are two sources of potable water for the City of Rohnert Park: City groundwater wells and Sonoma County Water Agency (SCWA) supply. One well (W-41), located south of Business Park Drive, approximately halfway between Labath Avenue and Redwood Drive, is connected to the distribution system on the west side of Highway 101. Per Table 4.1 of the *2010 Urban Water Management Plan (UWMP)*¹, the production rate for this well is 285 gallons per minute. There are no other wells (active, inactive, or standby) within one-half mile of the Plan Area. There is one SCWA turnout 163, connected to the distribution system on the west side of Highway 101. Per Tables 3.15 and 4.11 of the UWMP, there is sufficient capacity for all undeveloped areas contemplated for growth through 2035, with diversions from SCWA increasing while groundwater pumping decreasing to meet these anticipated demands. According to the *Water Model Study Summary Report (WMSSR)*², piping improvements to SCWA turnout 163 will be needed to provide adequate water supply to the Plan Area.

CONVEYANCE/STORAGE

According to the WMSSR, the City has approximately 4.2 MG of storage; an additional 640,000 gallons of storage will be needed to provide adequate water storage for the Plan Area. This does not reflect an additional 6.0 MGD that is assumed to be available based upon available water stored in SCWA tanks. The nearest City operated above-ground storage tank (Tank T-7) is located southwest of the Plan Area.

¹ Prepared by Winzler and Kelly, June 2011.

² Prepared by Brelje & Race, September 2004.

DISTRIBUTION

There are existing 8-inch water mains located to the south of the Plan Area along Business Park Drive and east of the Plan Area along Dowdell Avenue and Redwood Drive. In addition, the recent widening of Golf Course Drive West include a 12-inch water main to the west boundary of the Plan Area, with 8-inch stubs at Langner and Labath Avenues. A 12-inch main is also planned to extended northward on Dowdell Avenue to connect to the existing main behind the Home Depot. There are no water mains along the north or west side of the Plan Area. According to the WMSSR, a new 8-inch water main will need to be constructed along Redwood Drive, starting at the SCWA turnout and continuing south for 350 feet.

In addition to the potable water mains, an 18-inch recycled water main intersects the Plan Area along Golf Course Drive West, from Langner Avenue to Dowdell Avenue.

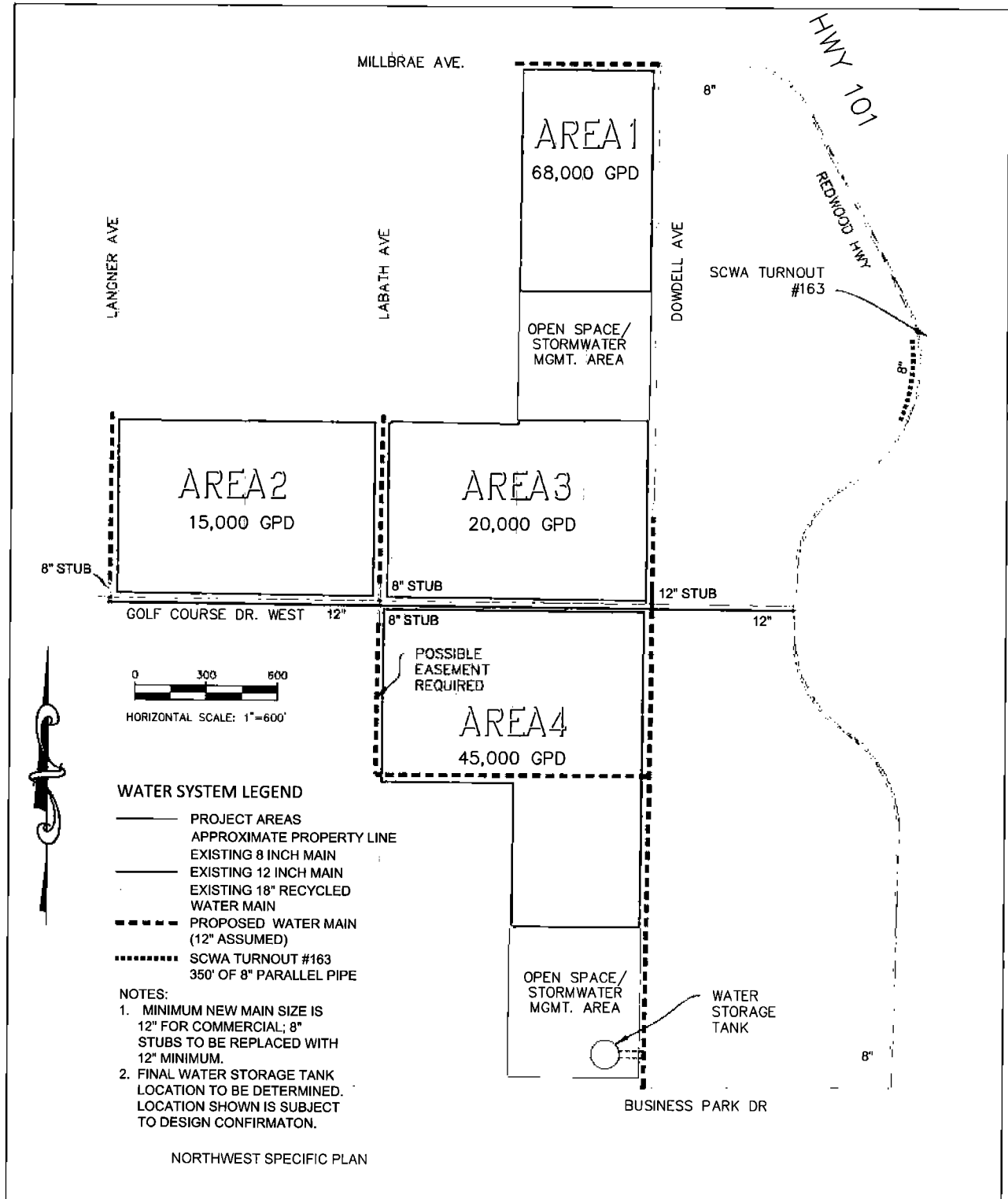
ESTIMATION OF WATER DEMAND FOR NORTHWEST AREA

All new development within the City is expected to achieve significant levels of water conservation by using new, high efficiency appliances. By utilizing these measures, the estimated water demand for the Plan Area is approximately 0.148 mgd for both the multi-family and non-residential uses. The water demands for each of the four areas are shown on Figure 7-5.

Usage per multi-family unit was estimated at 92 gallons per day. Significant assumptions used in the calculation of multi-family usage are:

- All irrigation demands would be met with reclaimed water.
- Indoor usage, before conservation measures, per Maddaus report of 54 gallons per capita per day (gpcd).
- Per Maddaus report, 2.4 persons per unit.
- Four conservation measures implemented to further reduce usage:
 - **ND 3** – High efficiency toilets (50 percent water usage reduction): Savings estimated at 7.2 gallons per person per day.
 - **ND 4** – High efficiency dishwasher (34 percent water usage reduction): Savings estimated at 0.3 gallon per person per day.
 - **ND 5** – High efficiency clotheswasher (50 percent water usage reduction): Savings estimated at 5.9 gallons per person per day.
 - **ND 7** – High efficiency faucets and showerheads (15 percent water usage reduction): Savings estimated at 2.6 gallons per person per day.

Figure 7-5: WATER DEMAND



Source: Coastland Civil Engineering, 2014.

In order to determine the percentage of water used by fixture type in the non-residential uses, the study titled “Commercial and Institutional End Uses of Water”, prepared by the AWWA Research Foundation in 2000; was utilized. These percentages were then applied to the water usage reduction percentages by fixture type discussed above. Other noteworthy assumptions used in the calculation are:

- All irrigation demands would be met with reclaimed water.
- Hotel usage of 90 gallons per occupied room per day, with 80 percent occupancy rate.
- Hotel domestic plumbing usage of 24 percent, with ND 3 and ND 7 applicable for hotel usage.
- In hotels, 10 percent of usage from toilets and 90 percent of usage from faucets and showerheads.
- Office usage of 12 gallons per square foot per year.
- Office, retail, and light industrial domestic plumbing usage of 37 percent, with ND 3 applicable for usage.
- Retail usage of 0.11 gallons per day per square foot.
- Light industrial usage of 0.62 gallons per day per square foot (medical offices).

Calculations of water usage and water conserved are included in Appendix C.

SUMMARY OF IMPROVEMENTS

City standards require that water mains in all commercial development areas are a minimum size of 12 inches, and water main sizing is primarily based on fire flow standards of 3,000 gallons per minute (gpm) for commercial uses. The total length of 12-inch water mains presented in Figure 7-5 is estimated at almost 1.2 miles. Other improvements include a 640,000-gallon water storage tank and a new, parallel 350 linear foot, 8-inch water main from the SWCA turnout identified in the 2004 *Water Model Study Summary Report*. The water main in Dowdell Avenue is included in the PFFP and is subject to a Fee Credit.

The water demand increases will trigger the need for significant water improvements, as existing facilities will be undersized at build-out or do not presently exist. The required minimum improvements are identified as:

- 1.2 linear miles of 12-inch water main
- One water storage tank (at 640,000 gallons)
- 350 linear feet of 8-inch water main

ESTIMATED COSTS OF IMPROVEMENTS

Based on the above summary of needed water improvements, planning level estimates of probable construction costs were prepared. Recent bid information for nearby water main improvement projects was evaluated. Per linear foot cost for the 12-inch water mains were estimated at \$265. Recent estimates on water storage tanks of the size proposed is about \$1.12 Million. Both include a multiplier of 1.35 to re-

flect costs associated with engineering, environmental, construction management, and contingency. The per-linear-foot cost includes other ancillary costs, such as valves, fire hydrants (with valves), tie-ins, and re-connections for services. The new, parallel 350 linear foot, 8-inch water main is estimated at \$200,000. The estimated total cost of water system improvements is \$4.1 Million. Note these costs are for major trunk mains and storage, and do not reflect costs for internal water or fire protection lines, or for water services to individual buildings.

WASTEWATER PLAN

This section briefly discusses the two facets that are included in the wastewater system: treatment/disposal and collection (mains and pump/lift stations); and the proposed wastewater system improvements for the Plan Area. For the purposes of the utility assessment and recommendations, the Plan Area has been divided into four areas. Wastewater facilities are shown in Figure 7-6.

TREATMENT/DISPOSAL AND ALLOCATION CAPACITY

The City is one of five entities (City of Santa Rosa, City of Rohnert Park, City of Sebastopol, City of Cotati, and the South Park County Sanitation District) that comprise the Santa Rosa Subregional Sewer System. All wastewater treatment occurs at the Laguna Water Reclamation Plant located in Santa Rosa; the City of Rohnert Park does not treat any sewage.

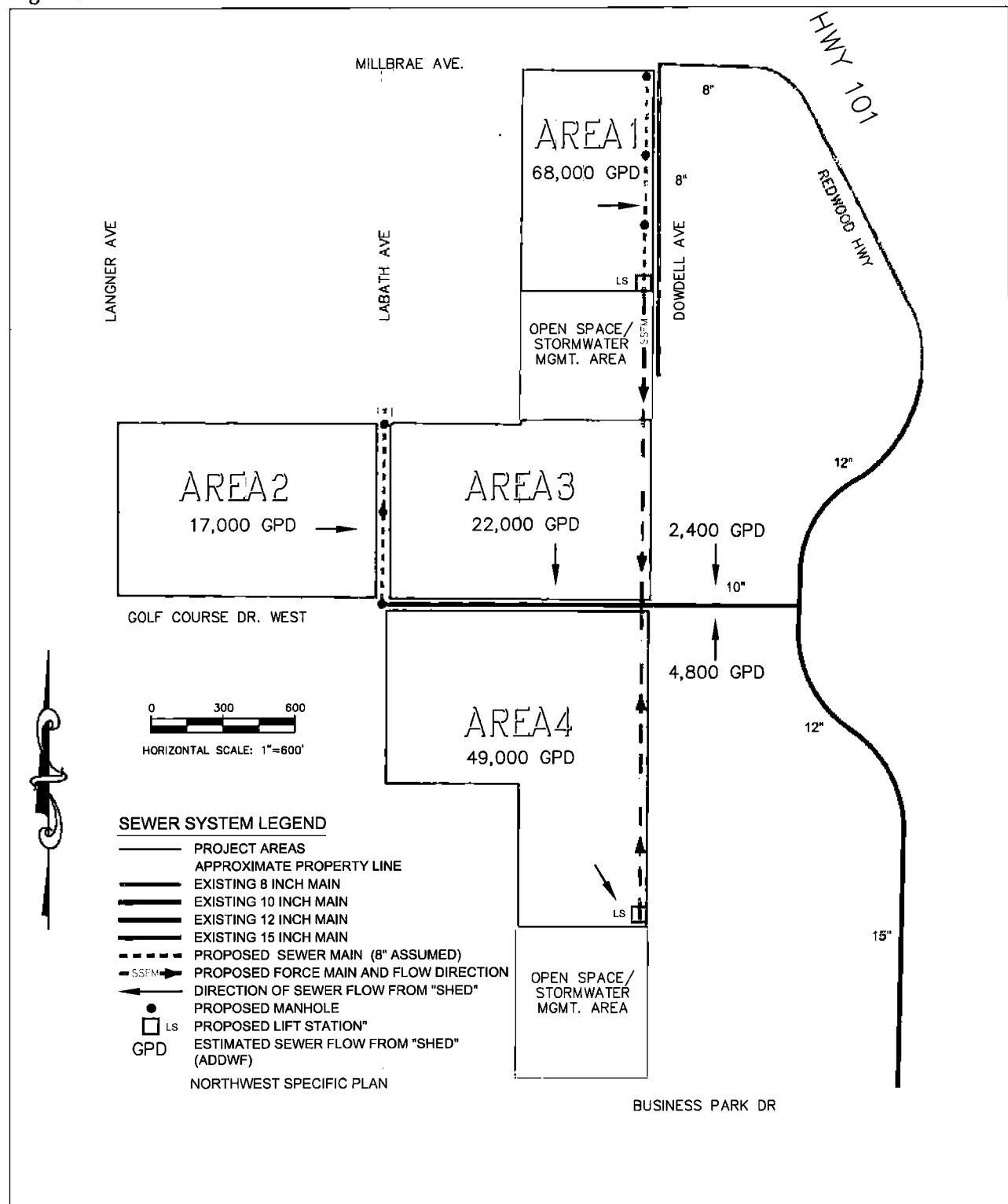
The Fifth Amendment to the Agreement for Use of the Santa Rosa Subregional Sewerage System (executed November 19, 2008) identified the allocation of treatment capacity to all of the entities. The City's treatment allocation is 5.08 MGD average dry weather flow (ADWF) and disposal allocation as 4.15 mgd. For calendar year 2011, ADWF (period of time not influenced by rainfall) was estimated at 2.85+/- MGD. The City has adequate reserve in their allocation to accommodate wastewater from the Plan Area.³

COLLECTION

There are existing 8-inch, 12-inch, and 15-inch sewer mains that serve the commercial areas to the east of the Plan Area. An 8-inch main exists along Dowdell Avenue and extends northerly approximately 600 linear feet to the intersection of Dowdell Avenue and Millbrae Avenue. This 8-inch main continues easterly along Millbrae Avenue, and then continues southerly along Redwood Drive. The sewer main size increases to 12 inches approximately 230 feet northerly of the intersection of Redwood Drive and Commerce Drive. The sewer main continues south, and the main size increases to 15 inches at the intersection of Golf Course Drive West and Redwood Drive. The 15-inch sewer main continues south to an existing lift station on J. Rogers Lane. The City recently extended a 10-inch sewer main west on Golf Course Drive West from the intersection of Dowdell Avenue to the intersection of Labath Avenue.

³ Conference call with Coastland Civil Engineering, Inc. and City Engineer, Patrick Barnes, on July 5, 2012.

Figure 7-6: WASTEWATER FACILITIES



Source: Coastland Civil Engineering, 2014.

There are no sewer mains along the north or west side of the Plan Area. The plans for the casino development depicted an 8-inch force main running eastwardly along Business Park Drive to the lift station on J. Rogers Lane. The City's force main discharge to the City of Santa Rosa Laguna Treatment Plant runs northwardly along Labath Avenue from Business Park Drive, then westerly along Golf Course Drive West. Per the Public Facilities Finance Plan, the existing lift station will need to be expanded and the costs will be covered by connection fees.⁴ Furthermore, up to two new lift stations will likely be needed within the Plan Area.

ESTIMATION OF WASTEWATER FLOWS FOR NORTHWEST AREA

The City serves as the wastewater provider within the Plan Area. As such, the City is responsible for operation and maintenance of the collection system. Wastewater is collected from individual services into the City's collection system.

For wastewater flows, the assumption was made that all domestic water usage would be sent to sewer. These demands were increased for peaking factors and inflow/infiltration per City direction. The wastewater flows for each of the four areas are shown on Figure 7-6.

SUMMARY OF IMPROVEMENTS

The City modeled the existing 8-inch and 12-inch sewer main along Dowdell Avenue and Redwood Highway north of Golf Course Drive West to determine if sewer flows from Area 1 could be sent to this sewer main. The analysis indicated that peak flows exceed City standards in the 8-inch main segments. Therefore, per City direction, all flows from the Plan Area would be sent to the 10-inch main constructed in Golf Course Drive West. In order to serve the four subareas, a total of three 8-inch mains are proposed with a total length of 4,400 linear feet. The sewer main in Dowdell Avenue is also included in the PFFP and is subject to a Fee Credit.

The City also modeled the 12-inch and 15-inch main south of Golf Course Drive West. The model indicated that all flows from the Plan Area could be accommodated in the existing 12-inch and 15-inch mains on Redwood Avenue, south of Golf Course Drive West; therefore, no downstream improvements are proposed.

City standards allow for sewer mains with a minimum slope of 0.005 ft/ft. As discussed above, the sewer mains on Dowdell Avenue, north and south of Golf Course Drive West, will tie into the new 10-inch main on Golf Course Drive West. This main is approximately 11 feet below ground surface at the point of connection. Based on the length of the northern sewer main, a lift station will be required. A lift station may not be needed on the southern sewer main, but for the purposes of this study, one is assumed given the unknowns about building lateral depth, site topography, and other related issues such as potential utility conflicts.

⁴ Conference call with Coastland Civil Engineering, Inc. and City Engineer, Patrick Barnes, on July 5, 2012.

The water demand increases will trigger the need for significant wastewater improvements, as existing facilities will be undersized at build-out or do not presently exist. The needed improvements are identified as:

- 4,400 linear feet of 8-inch sewer main
- One or two sewer pump stations
- One reclaimed water booster pump station and storage tank

ESTIMATED COSTS

Based on the above summary of needed wastewater improvements, planning level estimates of probable construction costs were prepared. Recent bid information for nearby wastewater main improvement projects was evaluated, and feedback from a local contractor that has completed projects in the Plan Area was obtained. Per linear foot costs for 8-inch wastewater mains was estimated at \$281. Recent lift station project estimates were also evaluated, and a cost of \$400,000 is estimated for each lift station. This includes a multiplier of 1.35 to reflect costs associated with engineering, environmental, construction management, and contingency. This per linear foot costs includes other ancillary costs, such as sewer manholes and reconnections for services. The estimated total cost of sewer system improvements is \$2.0 million. Note these costs are for the major trunk mains and lift stations, and do not reflect costs of internal/site sewer lines that may be required within the areas or laterals to individual buildings.

RECLAIMED WATER SYSTEMS PLAN

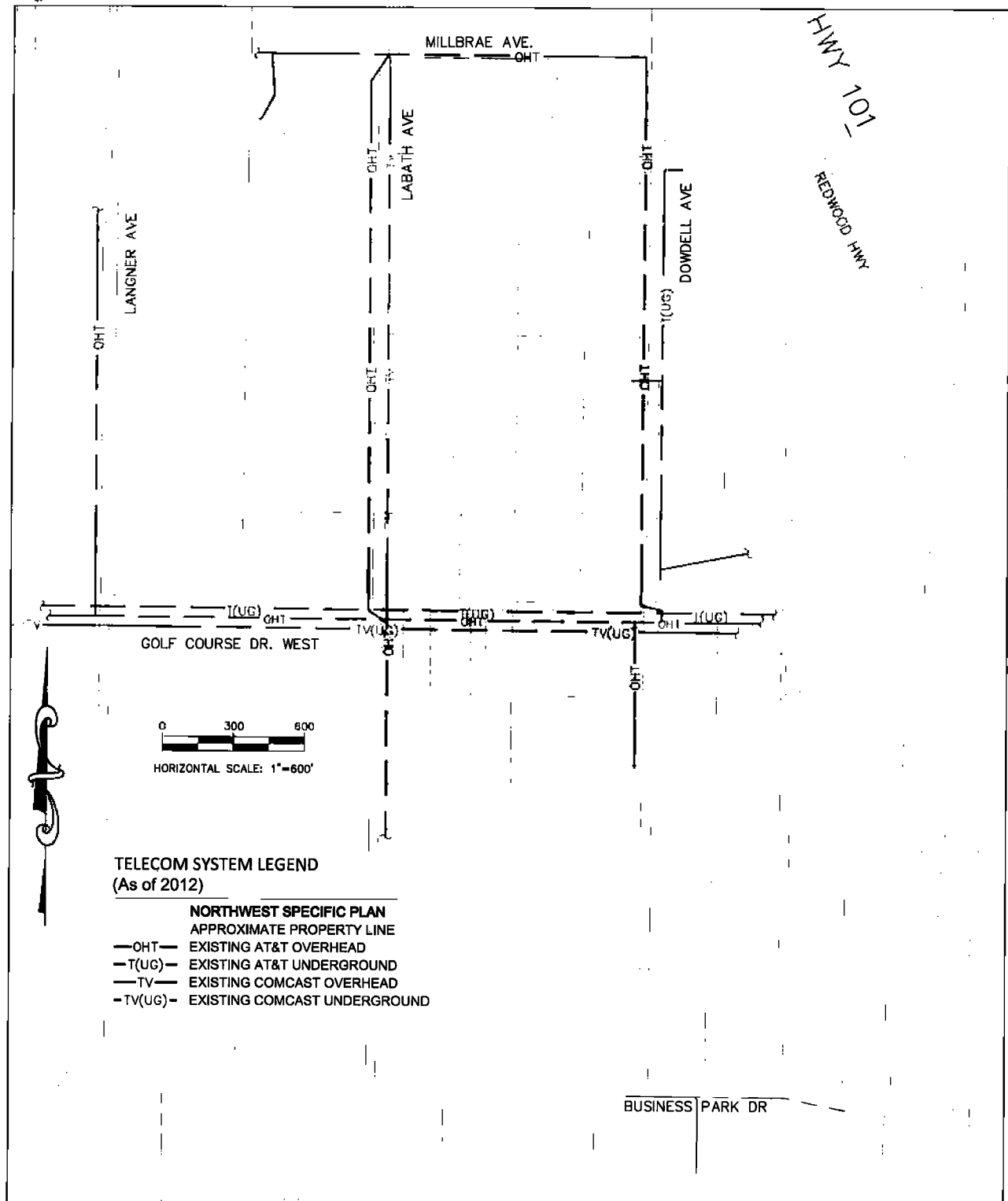
Based on discussions with City staff, pressures for the existing reclaimed water main in Golf Course Drive West are very low. Therefore, a booster pump station and storage will be needed for the irrigation system for the Plan Area. A formal analysis of an irrigation system, and hence a reclaimed water internal distribution system, is not within the scope of this study. However, it is assumed that a small tank and booster pump station will be necessary, and costs for this facility will be approximately \$200,000, including soft costs.

In addition to the water demand increases triggering the need for significant wastewater improvements, there is a desire to improve the City's use of reclaimed water. One reclaimed water booster pump station and storage tank would be needed to improve the city's reclaimed water.

DRY UTILITY SYSTEMS PLAN

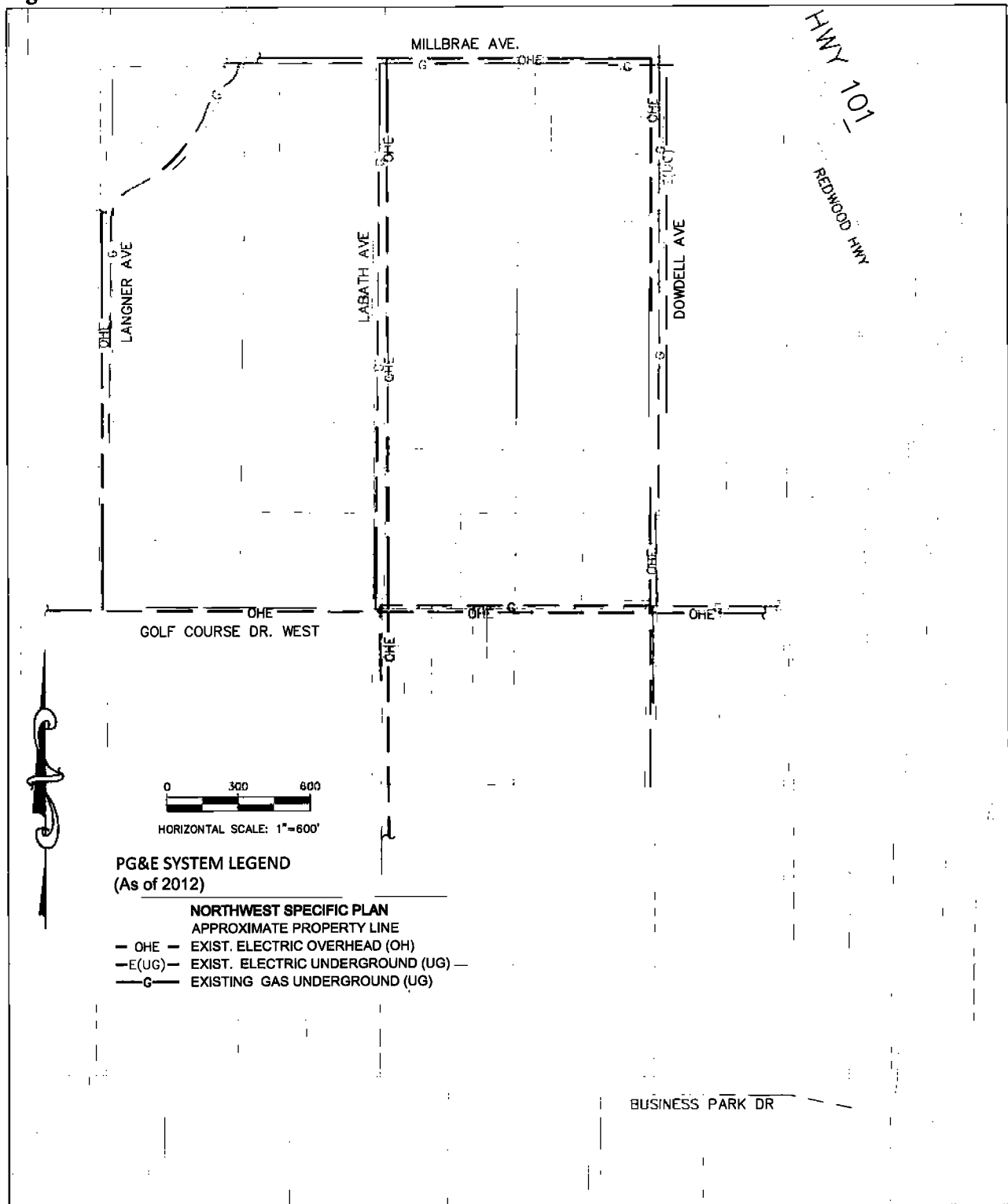
This section briefly discusses the existing cable and telecommunication systems and improvements in the Plan Area. Comcast and AT&T provide telecommunication, cable television, and internet services; infrastructure is located both above ground on utility poles and below ground. PG&E provides electric and natural gas services. Electrical infrastructure in the Plan Area is located above ground on utility poles, as well as below ground. Natural gas pipelines are belowground. In addition, dry utilities were under construction in Golf Course Drive West at the time of this report. See Figure 7-7 and 7-8 for dry utilities.

Figure 7-7: **EXISTING TELECOM IMPROVEMENTS**



Source: Coastland Civil Engineering, 2014.

Figure 7-8: EXISTING PG&E IMPROVEMENTS



Source: Coastland Civil Engineering, 2014.

AT&T

AT&T has primarily aerial facilities in the Plan Area, which are located along Golf Course Drive West, Labath Avenue, and the west side of Dowdell Avenue. There are underground facilities along the east side of Dowdell Avenue between Millbrae Avenue and Golf Course Drive West, which serve the commercial developments east of Dowdell Avenue. Existing telecom facilities are shown in Figure 7-7.

AT&T has indicated that as soon as the type of proposed development is known, or preliminary plans are available, they can review and comment on the system. They have stated that it would be difficult to do a capacity review at this time, without any knowledge of the type and extent of plans.

COMCAST

Comcast has minimal capacity at present for expansion into the Plan Area. They would extend service into the area if it was cost-efficient. For example, if residential development occurs, they would extend the system as the land is developed. For commercial, they would look at who is requesting the service, how far it is from the existing system, how many customers are requesting service, and what it would cost to extend the system. Comcast only has ring fiber in the area now and it could not be used to service any facilities. Ring fiber carries traffic from hub to hub in each town or city and serves as a redundant path in the event of a cable being cut. Separate fiber or coaxial lines would need to be extended to service the Plan Area.

The existing ring fiber is located underground from Business Park Drive, under the ditch to the first pole on Labath Avenue, and then continues overhead on Labath Avenue to Golf Course Drive West and to Millbrae Avenue. The fiber is also located underground on Redwood Drive at Golf Course Drive West and then runs underground westerly on Golf Course Drive West to Langner Avenue. There are no facilities on Dowdell Avenue.

Proposed development of the Plan Area would likely involve new telecommunication facilities being installed underground.

GAS & ELECTRIC SYSTEMS

Electrical infrastructure in the Plan Area is located primarily above ground on utility poles. There are some underground facilities along the east side of Dowdell Avenue. Existing PG&E facilities are shown in Figure 7-8. The following is a summary of the current electrical facilities:

- **Millbrae Avenue.** 12 kilovolts (kV), 3-phase on north side on joint poles.
- **Dowdell Avenue.** 21 kV, 3-phase underground on the east side from Millbrae Avenue to the Home Depot; single phase on the west side on joint poles. South of Home Depot, there is 12 kV, single phase along the west side on joint poles to the south terminus south of Golf Course Drive West.

- **Golf Course Drive West.** 12 kV, single phase on south side on joint poles. Line terminates west of Labath Avenue.
- **Labath Avenue.** 12 kV, 3-phase on east side on joint poles.
- **Business Park Drive.** 21 kV, 3-phase underground along the south side.

Natural gas infrastructure is as follows:

- **Millbrae Avenue.** 2-inch line along the south side of the street.
- **Dowdell Avenue.** Mostly 4-inch line along the east side from Millbrae Avenue to north of Golf Course Drive West; 2-inch line southward across Golf Course Drive West to the terminus south of Golf Course Drive West.
- **Golf Course Drive West.** 2-inch line along the south side of the street between Dowdell Avenue and Labath Avenue; none exists west of Labath Avenue. During the recent improvements to Golf Course Drive West, the 2-inch main was replaced to Labath Avenue, but no main was extended west of Labath Avenue.
- **Labath Avenue.** 2-inch line along the west side of the street.
- **Business Park Drive.** 4-inch line along the south side of the street.

SUMMARY OF IMPROVEMENTS

Infrastructure improvements will be needed to adequately serve the Plan Area at build-out.⁵ These improvements would be determined on a project-specific basis and required to be constructed by the applicant or utility provider. In conjunction with development, it may be desirable to underground some of the existing overhead facilities to improve the aesthetics and reliability of the utilities.

According to PG&E, improvements are typically identified on a project-level (after service applications are submitted) and not on a planning-level basis.⁶ It is likely that upgrades to the single phase electrical infrastructure will be required, as well as undergrounding of the improvements and removal of the existing infrastructure serving the private residences. The 2-inch gas mains may not be sufficient to serve the development of the area. PG&E has, however, indicated that their overall “backbone” infrastructure in the vicinity is adequate.

Dry utility providers (AT&T, Comcast, and PG&E) indicated that infrastructure improvements will be needed to adequately serve the Plan Area at buildout. These improvements would be determined on a project-specific basis and required to be constructed by the applicant or utility provider. As such, no cost should be borne by the City. If there is sufficient demand, it is possible that there would be no cost to the applicant.

⁵ Conversations between Coastland Civil Engineering, Inc. and AT&T and Comcast representatives.

⁶ Coastland Civil Engineering, Inc. contacted the PG&E representative (Mike Miller) to discuss the process for planning improvements. His response indicated that information generated to date will provide for a broad description of improvements needed to serve the Area at build-out, but the current level of information is insufficient to ascertain specific improvements needed.

UTILITIES AND INFRASTRUCTURE POLICIES

This section provides an overview of utilities and infrastructure policies that should be followed by private developers and/or the City of Rohnert Park during implementation of this Specific Plan.

DRAINAGE

- Policy UTIL-1.1 Provide detention basins for each tributary area within the Specific Plan Area (Areas A through D) that can contain runoff from the 100-year storm and release it at flow rates that are less than pre-development conditions or do not exceed City or County storm drain design criteria in order to ensure that on-site or off-site properties are protected from flooding. The design of these detention facilities will be subject to the approval of the City of Rohnert Park, the Sonoma County Water Agency, and the North Coast Regional Water Quality Control Board. In the event that development occurs in separate phases over time, the project applicants shall demonstrate that each phase avoids any increases in peak storm flows and does not exceed the capacity of storm drainage facilities. Since the Specific Plan Area is located on very flat terrain, fill may be needed to elevate the individual parcels to gravity drain to vegetative swales or directly to the detention facilities. Alternatively, stormwater discharge from the parcels can be piped to the detention basins. Suitable areas of open space that can accommodate detention basins designed to contain runoff from the 100-year storms have been designated in each portion of the Specific Plan. Final design may require the shifting of detention facilities depending on the topography and/or the order of development of individual parcels within each tributary area. The basins have been designed to drain within 48 hours of the 100-year storm to provide vector control and also can be used for recreational opportunities during the dry season.
- Policy UTIL-1.2 Future development projects permitted under the Specific Plan shall be required to incorporate LID design features and BMPs into the site design, as per requirements of the MS4 permit and Sonoma County *Low Impact Development Technical Design Manual*.
- Policy UTIL-1.3 Provide the detention basins recommended in this Plan in the north and south portions of the Specific Plan Area to effectively capture and convey the runoff produced by the 100-year storm

so that post-development flow rates will be less than pre-development flow rates.

Policy UTIL-1.4 Provide a vegetated swale to convey runoff from the areas west and south of the detention basin to the basin in the designated area within the Specific Plan Area.

Policy UTIL-1.5 Coordinate with the City of Rohnert Park and Sonoma County Water Agency to determine the preferred discharge location.

WATER

Policy UTIL-1.6 Adhere to the Citywide policy that all new development within the City is expected to achieve significant levels of water conservation by using new, high-efficiency appliances.

Policy UTIL-1.7 All development in the Specific Plan Area shall meet or exceed City standards for water mains and fire flow standards.

WASTEWATER

Policy UTIL-1.8 Provide sufficient wastewater facilities to meet City standards prior to or during development to meet the water demand increases; existing facilities will be undersized at build-out or do not presently exist.

RECLAIMED WATER

Policy UTIL-1.9 Provide the necessary facilities to improve the City's use of reclaimed water.

Policy UTIL-1.10 All development in the Specific Plan Area shall meet their irrigation demands with reclaimed water.

DRY UTILITIES

Policy UTIL-1.11 All development in the Specific Plan Area shall include telecommunication facilities to meet the anticipated demand; wherever possible, these facilities shall be installed underground.

Policy UTIL-1.12 All dry utility improvements shall be determined on a project-specific basis and required to be constructed by the applicant or utility provider. As such, no cost should be borne by the City.

Chapter Eight

IMPLEMENTATION AND FINANCING

The vision for the Northwest Specific Plan will be developed over a period of twenty years or more. In order for the entire Plan Area to be developed in an orderly fashion, a program of implementation measures is necessary. This chapter outlines a financing program for the Plan Area.

Each property owner or developer will pay for and construct all on-site capital improvements and, when required elsewhere per Rohnert Park Municipal Code, dedicate those to the public. In addition, each property owner or developer will pay for their proposed development's fair share of off-site capital improvements in accordance with the 2011 Update to the Public Facilities Finance Plan (PFFP). This section of the Specific Plan describes how property owners and developers will pay for off-site capital improvements in the Plan Area when those improvements are not otherwise covered by the 2011 PFFP.

HOW TO USE THIS SECTION

The reader should use this to understand the array of financing and funding opportunities that are available for realizing the vision outlined in this Specific Plan. The reader should review this section to learn what potential partnering opportunities exist for financing new infrastructure and other improvements that will be required to support new development, and the costs of these new facilities.

PLAN AREA CAPITAL IMPROVEMENTS

The capital improvements listed below may serve more than one property or project in the Plan Area and would likely constitute an off-site improvement. The financing tools discussed in this section would apply to these Plan Area capital improvements. As summarized in Table 8-1 below, the preliminary estimate is that the identified Plan Area capital improvements could cost a total of \$12.7 million. These estimates are preliminary and are intended to provide an idea of the magnitude of capital investment needed to provide public facilities and services for the Plan Area.

Table 8-1: CAPITAL IMPROVEMENTS COST ESTIMATES

Improvement	North Side Share	South Side Share	Total Cost
Water/Sewer/Reclaimed Water	\$3,790,000	\$1,650,000	\$5,440,000
Road Improvements	\$2,745,000		\$2,745,000
Stormwater Detention	\$2,216,000	\$1,320,000	\$3,536,000
Land for Parks and Open Space	\$800,000	\$200,000	\$1,000,000
Total	\$9,551,000	\$3,170,000	\$12,721,000

WATER SYSTEM

As described in Chapter 7: Utilities and Infrastructure, buildout of the Specific Plan will require the installation of almost 1.2 miles of 12-inch water mains, as shown in Figure 7-5; a 640,000 gallon water storage tank; and a new parallel 350 linear foot 8-inch water main from the Sonoma County Water Agency (SCWA) Turnout. The preliminary cost estimate for water system improvements is \$3.2 million.

SEWER SYSTEM

As described in Chapter 7, buildout of the Specific Plan will require the installation of 4,400 linear feet of 8-inch sewer mains and two lift stations, as shown on Figure 7-6. The preliminary cost estimate for sewer system improvements is \$2 million.

RECLAIMED WATER SYSTEM

As described in Chapter 7, it is assumed that a small tank and booster pump station will be necessary for the reclaimed water system in the Plan Area. The preliminary cost estimate for the reclaimed water system improvements is \$200,000, including soft costs.

ROAD IMPROVEMENTS

As described in Chapter 4: Circulation, buildout of the Specific Plan will require improvements to approximately 750 feet of Labath Avenue; 750 feet of Langner Avenue; 550 feet of Milbrae Avenue; and 1,800 feet of Dowdell Avenue. The preliminary cost estimate for these road improvements is \$2,745,000. Other road improvements will be necessary for Golf Course Drive West and the remainder of Dowdell Avenue, but these improvements have already been included in the 2011 PFFP.

STORMWATER DETENTION

As described in Chapter 7, regional stormwater detention facilities will be required for buildout of the Specific Plan. Development of these facilities involves land acquisition for the basins, construction costs

for the basins, and the cost of the conveyance system from individual properties to the basins. The preliminary cost estimates for the stormwater facilities improvements is \$3.5 million.

PARKS AND TRAILS

The Specific Plan proposes about 5 acres for park facilities and trails, not including pedestrian and bicycle facilities associated with roads. The preliminary cost estimate for parks and trails is \$1 million. In addition to the capital costs for parks and recreation facilities, the Plan Area capital improvements may also include the long-term operations and maintenance costs for parks, trails, and recreation facilities in the Plan Area.

OTHER CAPITAL IMPROVEMENTS

As development begins and proceeds in the Plan Area, other necessary capital improvements may come to light. Although such improvements are not specified in this list and the necessity of such improvements might not have been known at the time the Specific Plan was prepared, the finance tools discussed in this section would apply to other unforeseen capital improvements.

PHASING

The Plan Area south of Golf Course Drive West, or the south district, is under unified control. In contrast, the north district (north of Golf Course Drive West) is split among many different property owners. With different property owners, development of the north part of the Plan Area will likely either be delayed because it would take time for a developer to acquire all of the separate properties or be accomplished in pieces as different owners decided to sell or develop their properties at different times.

There is no physical reason why the Specific Plan should be developed in phases. With the difference in ownership patterns on either side of Golf Course Drive West, though, it is likely that the south district will develop first and as a single project. It is also likely that the north district will develop later than the area to the south and develop as several unrelated projects.

Accounting for the likely different development scenarios in the Plan Area is a key to how the Plan Area capital improvements may be financed. Two illustrative examples are described below.

- **Master Community Facilities District (CFD).** Because it is likely that the south district will develop first, one scenario is that the property owner(s) in this area would establish a CFD. The CFD would sell bonds to buy the land needed for and to construct the stormwater detention facilities. Each year, the property owners in the CFD would pay an additional property tax to repay the bonds. At some point later in time, the CFD could be expended to include some of the properties on the north side of Golf Course Drive West. It would be the same CFD, and it could once again sell bonds to buy the land for and construct a stormwater detention facility. But now, the detention facility would serve the properties on the north side, and only those properties would pay the addi-

tional property tax necessary to repay the bonds.

- **Northside CFD.** It might be reasonable to expect that some of the properties on the north side of Golf Course Drive want to develop before the other properties on the north side. Those wanting to develop could petition to establish a CFD that includes their properties as well as some of the other properties needed to generate sufficient revenue to repay the bonds. As long as the vote on the CFD receives two-thirds approval, the CFD would be established and include some properties not ready to development. Nevertheless, all of the properties within the district would have to pay an additional property tax to repay bonds sold to finance a stormwater detention facility.

POSSIBLE FINANCING MECHANISMS

It is the City's intent that the development allowed under the Specific Plan pay the cost of the Plan Area capital improvements. While the City may consider any and all financing mechanisms and tools authorized under State law, the most likely financing tools are described below.

DEVELOPMENT AGREEMENTS

The City will require a development agreement for any and all development projects proposed under the Specific Plan, pursuant to Chapter 17.21 of the Rohnert Park Municipal Code. The development agreement will stipulate how the proposed project will pay its fair share of the Plan Area capital improvements and ensure that the proposed project will be served by adequate public infrastructure and services, among other issues identified in the Municipal Code. The issues to be addressed in a development agreement may include, but not be limited to, the following:

- Water tanks
- Detention basins
- Reimbursement of landowners for parks, drainage, and other infrastructure
- Affordable housing
- Mitigation and other impact fees

DEVELOPMENT IMPACT FEES

The City charges development impact fees pursuant to the 2011 Update to the PFFP. The City may amend the 2011 PFFP subsequent to adoption of the Specific Plan, and such amendment(s) may impose additional impact fees to cover Plan Area capital improvement identified in the Specific Plan.

DEVELOPER-FUNDED IMPROVEMENTS

In some cases, the development of one or more parcels in the Specific Plan Area may require the construction of off-site infrastructure improvements, the size of which may be larger than what is needed to serve just the proposed development. In such cases, if the City does not have the funds available to pay for the additional infrastructure capacity, the property owner or developer may agree, through a develop-

ment agreement, to pay for the full cost of the off-site infrastructure improvement and to be repaid as additional development occurs. The development agreement would stipulate the terms of such repayment.

If the cost of infrastructure improvements is to be repaid to a property owner, developer, or other entity that paid the upfront cost, the City may prohibit other development under the Specific Plan until the project applicant has paid the required infrastructure cost repayment fee in accordance with the terms of the development agreement.

ASSESSMENT DISTRICTS

Assessment districts can finance the construction of public improvements on public property, public rights-of-way, and public easements. The public must pay for the portions of the improvements that provide general benefit to the public at large, but real property that receives a special benefit may be assessed for the costs, proportional to the level of benefit received.

Three different provisions of State law authorize assessment:

- Improvement Bond Act of 1915 (Streets and Highways Code Sections 8500 et seq.)
- Improvement Act of 1911 (Streets and Highways Code Sections 5000 et seq.)
- Municipal Improvement Act of 1913 (Streets and Highways Code Sections 10000 et seq.), which contains only provisions for establishing assessment districts

Assessment districts are intended to finance construction of physical improvements. They cannot pay for operations and maintenance, or additional services. If additional improvements are desired after an assessment district is established, the entire process is required for those additional improvements.

Assessment districts may be used to finance improvements in one of two general ways. The assessments may repay the City or an initial developer for the upfront costs of infrastructure. The City may also issue bonds pursuant to an assessment district and use the proceeds to fund the infrastructure improvements and use the assessments to repay the bonds.

Generally, assessment districts can be used to finance the following improvements:

- Local streets
- Streetlights
- Parks
- Landscaping
- Sidewalks
- Sanitary sewers
- Water supply and distribution facilities

- Gas and electric power
- Flood control and drainage improvements
- Parking facilities

The City has formed an assessment district under the auspices of the Act of 113 to secure new developments' share of its Interceptor Outfall project. This existing fixed lien has been placed on property within the Northeast Specific Plan Area, the University District Specific Plan Area, the Southeast Specific Plan Area, and the Stadium Lands and Sonoma Mountain Village Planned Development.

The City may form one or more assessment districts for the Northwest Specific Plan Area to pay for the costs of the Plan Area capital improvements. Establishment of an assessment district cannot occur if a majority of the affected property owners (weighted by the value of the proposed assessment) object. Assessment districts might be a viable method to fund off-site infrastructure improvements in the south district. They could prove to be difficult to use in the north district, though, because the area has multiple property owners who might not be willing to develop their property at the same time and/or willing to begin paying for infrastructure improvements at the same time. Nevertheless, under the authorizing legislation, the establishment of an assessment district in a portion of the Plan Area may include properties not proposed for development as long as a majority of the property owners (weighted by the value of the proposed assessment) do not object. The City may propose creative district boundaries to ensure that property within the Plan Area pays for the cost of needed infrastructure.

COMMUNITY FACILITIES DISTRICTS

Community Facilities Districts (CFDs) can fund the planning, design, purchase, construction, expansion, improvement, or rehabilitation of capital facilities, defined as having a useful life of five or more years. CFDs can also fund the provision of a variety of public services, such as public safety, parks and recreation, schools, library and cultural facilities, landscape maintenance and lighting, flood control, and site remediation.

The Mello-Roos Community Facilities Act of 1982 (Government Code section 53311 et seq.) authorizes the establishment of CFDs. However, these districts are not assessment districts, as discussed in the previous section, and differ in several important ways.

CFDs levy a special tax instead of a special assessment. This tax may be applied to the value of each property, rather than assessed based on the level of special benefit received. However, because it is a special tax, a two-thirds majority vote is required to approve the levy of the special tax. If the district has twelve or more registered voters, the election polls voters with each having an equal vote. If there are less than twelve registered voters, the election polls property owners with each vote weighted by acreage owned within the district boundary. Properties within the district need not necessarily be contiguous. Finally, establishing a CFD requires only a general description of the facilities, services, and costs associated with the district, not the detailed engineer's report required for assessment districts.

CFDs may fund the construction of the following types of facilities:

- Local park, recreation, parkway, and open-space facilities.
- Elementary and secondary school sites and structures.
- Libraries.
- Childcare facilities.
- Transmission/distribution facilities for water, natural gas, telephone, electrical energy, and cable television.
- Flood and storm protection, and storm drainage facilities.
- Other governmental facilities the legislative body creating the district is authorized by law to contribute revenue toward, construct, own, or operate.
- Work to bring public or private buildings or real property into compliance with seismic safety standards and regulations.

CFDs may also fund the following types of services:

- Police protection services.
- Fire protection and suppression services, and ambulance and paramedic services.
- Recreation program services, library services, maintenance services for elementary and secondary school sites and structures, and the operation and maintenance of museums and cultural facilities.
- Maintenance of parks, parkways, and open space.
- Flood and storm protection services including, but not limited to, the operation and maintenance of storm drainage systems and sandstorm protection systems.
- Removal or remedial action services for the cleanup of any hazardous substance released or threatened to be released into the environment.

When the Specific Plan was prepared, there were fewer than twelve registered voters in the south district. Establishment of a CFD in this area could be accomplished with a simple vote by the property owners. In contrast, the north district included more than twelve registered voters. Establishing a single CFD for the north district or for the entire Plan Area would require a two-thirds affirmative vote by registered voters residing in the proposed district.

The City may choose to seek to establish a CFD for a portion of the Plan Area and define the district area to include less than 12 registered voters in order to simplify the process of establishing the district. The City could also choose to seek to establish a CFD for all or a portion of the Plan Area and define the district area to include more than twelve registered voters, if the City believes that resident registered voters might be more likely to approve the CFD than property owners.

The City could establish multiple CFDs across the Plan Area to accommodate phased growth. The City could also establish one CFD, and require expansion of or annexation into the single CFD for later development.

LANDSCAPING AND LIGHTING MAINTENANCE DISTRICTS

Landscaping and lighting maintenance districts (LLMD) can fund the construction of certain public improvements and the operation and maintenance of public improvements. LLMDs are authorized by the Landscaping and Lighting Act of 1972 (Streets and Highways Code section 22500 et seq.).

An LLMD requires annual assessment process for any assessments other than previously approved assessments to pay previously approved and issued debt. The annual assessment process is similar to that used to establish assessment districts. But the majority protest only matters if the proposed assessment is an increase.

The improvements and services provided by LLMDs include:

- Landscaping.
- Statuary, fountains, and other ornamental structures.
- Public lighting, including traffic signals.
- Appurtenant facilities, including grading, clearing, removal of debris, the installation or construction of curbs, gutters, walls, sidewalks, or paving, or water, irrigation, drainage, or electrical facilities.
- Park or recreational improvements.
- Land preparation.
- Lights, playground equipment, play courts, and public restrooms.
- The maintenance or servicing, or both, of any of the foregoing.
- Acquisition of land for park, recreational, or open-space purposes.
- Acquisition of existing improvements.
- Acquisition or construction of any community center, municipal auditorium or hall, or similar public facility for the indoor presentation of performances, shows, stage productions, fairs, conventions, exhibitions, pageants, meetings, parties, or other group events, activities, or functions, whether those events, activities, or functions are public or private.

LLMDs can be expanded over time, following a process similar to that used to establish the district. For instance, the initial LLMD could incorporate the south district, and the LLMD could be expanded to provide improvements and ongoing operations and maintenance as areas proposed for development in the north district.

An LLMD would be well suited to the operation and maintenance of parks and recreational and trail facilities within the Plan Area. In the absence of an agreement to form or join an LLMD, the City may require a proposed development to create a homeowners association to own, maintain, operate, and assume liability for required parks and recreation facilities.

FINANCING REQUIREMENTS

It is the intent of the Specific Plan that property owners and developers within the Plan Area pay the full cost of the Plan Area capital improvements. The City may prohibit development under the Specific Plan in the absence of adequate infrastructure or in the absence of a means to pay for the necessary infrastructure. The implementation of possible financing mechanisms may require some property owners to begin paying for infrastructure improvements prior to the time when they might choose to develop their property. The use of the possible financing mechanisms may result in higher costs to property owners and developers that delay proceeding with development. Through a development agreement, the City may accept the dedication of land for rights-of-way, easements, or other public use as pre-payment for future development impact fees, special assessment levies, or special taxes. The City may condition annexations into the City's jurisdiction on parallel annexations into new or existing CFDs, LLMDs, or other funding or financing mechanisms intended to pay for off-site infrastructure and public improvements.

Exhibit A
Resolution 2014-157

City of Rohnert Park **NORTHWEST SPECIFIC PLAN**



Adopted November 25, 2014
City Council Resolution No. 2014-157



EXHIBIT 7: TEXT REVISIONS TO NORTHWEST SPECIFIC PLAN

ally discharging into an outfall for the Bellevue-Wilfred Flood Control Channel. This storm drain was designed to handle flow from areas east of Dowdell Avenue and was not sized to handle runoff from the Plan Area. However, there are several existing inlets on the west side of Dowdell Avenue that connect to lateral pipes that, in turn, connect to the 36-inch storm drain. Future street improvements along the west side of Dowdell Avenue should be able to discharge to this storm drain. However, this system will not be able to accommodate runoff from the future development within Area D.

Existing drainage conditions are shown on Figure 7-1. Calculations were performed to determine peak runoff rates for existing and proposed development conditions for each tributary area. The detailed calculations are provided in Appendix A and summarized in Table 7-3.

Table 7-3: PEAK RUNOFF RATES

Area	Location	Area (acres)	Existing 10-Year Storm (cfs)	Proposed 10-Year Storm (cfs)	Existing 100-Year Storm (cfs)	Proposed 100-Year Storm (cfs)
A	South Half, Drains South to Business Park Drive	24.7	13.5	49.4	18.9	69.3
B	South Half, Drains North to Golf Course Drive West	9.7	5.3	20.9	7.4	29.3
C	North Half – Drains South to Golf Course Drive West	38.2	20.8	77.6	29.2	108.8
D	North Half – Drains North to Millbrae Avenue	18.2	9.9	37.5	13.9	52.6

PROPOSED SITE DRAINAGE

The drainage plan for the Plan Area will preserve the four tributary area boundaries and discharge points described in the previous section. Construction of detention ponds and in some cases off-site drainage improvements will be needed for each tributary area. The Conceptual Drainage Plan describes the applicable design criteria and provides an example of a drainage plan for the Plan Area that would meet the applicable design criteria and preserve the four tributary area boundaries and discharge points characterizing the Plan Area. The location of the detention basins and storm drainage infrastructure is described for conceptual purposes only and the ultimate size and location of the basins and storm drain lines will be determined based on proposed specific development proposals and coordination between landowners. Readers should use the Proposed Site Drainage section of this Conceptual Drainage Plan as an example of the type of system wide improvements that may be considered within the Plan Area.

The proposed project will result in a change in land uses from agricultural and rural residential to mixed use, commercial, and flex, which will result in an increase in impervious surfaces and peak runoff rates

tional property tax necessary to repay the bonds.

- **Northside CFD.** It might be reasonable to expect that some of the properties on the north side of Golf Course Drive want to develop before the other properties on the north side. Those wanting to develop could petition to establish a CFD that includes their properties as well as some of the other properties needed to generate sufficient revenue to repay the bonds. As long as the vote on the CFD receives two-thirds approval, the CFD would be established and include some properties not ready to development. Nevertheless, all of the properties within the district would have to pay an additional property tax to repay bonds sold to finance a stormwater detention facility.

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